

REHABILITATION

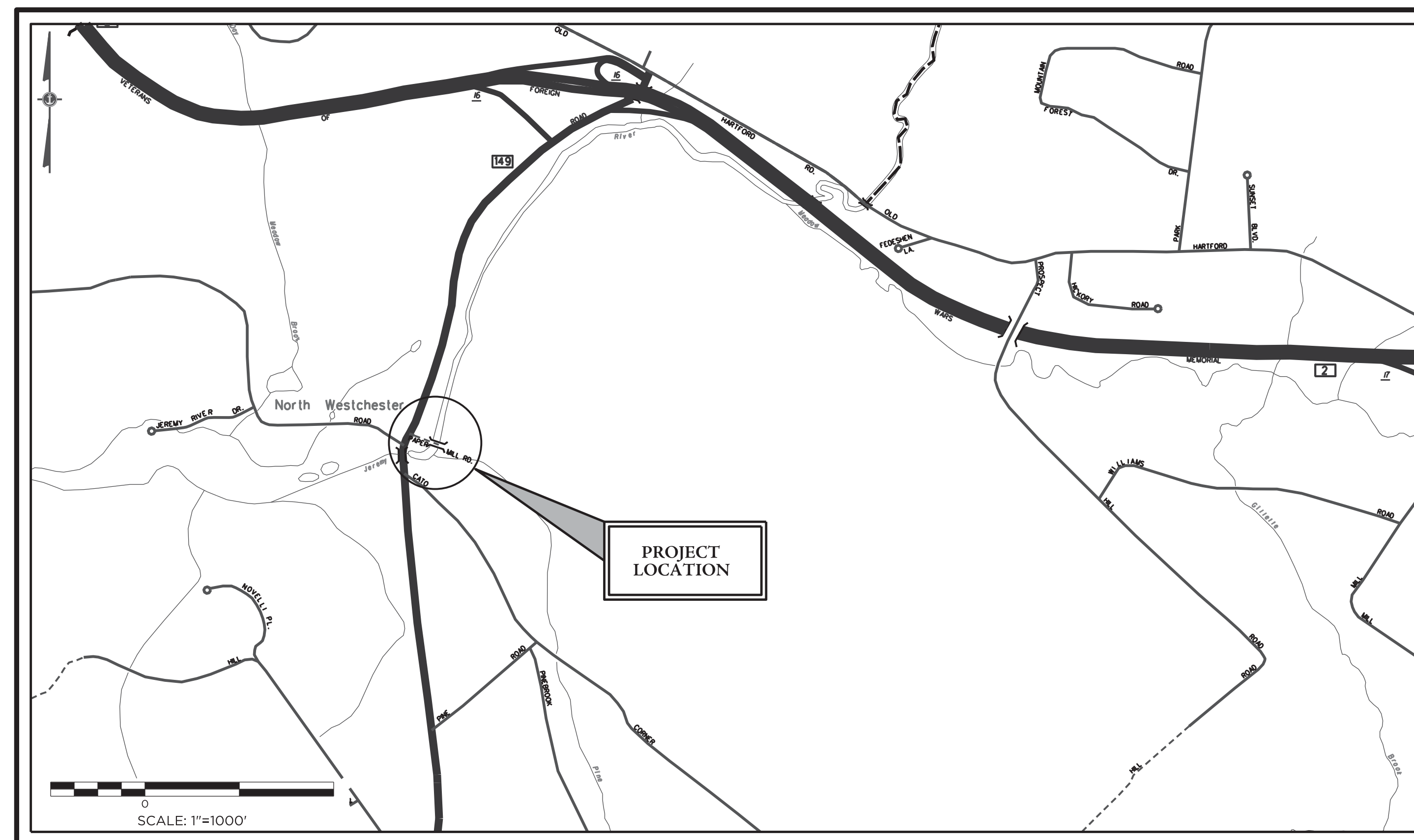
STATE PROJECT NO. 9028-5528

OF

PAPER MILL RD. BRIDGE NO. 05528 OVER JEREMY RIVER COLCHESTER, CT

PREPARED FOR

TOWN OF COLCHESTER
MARY BYLONE, FIRST SELECTMAN



LOCATION MAP
SCALE: 1" = 1000'

LIST OF SHEETS

DATE: 02/11/20
REVISED:

ROADWAY PLAN & PROFILE	1
GENERAL PLAN	2
LAYOUT PLAN	3
STRUCTURE DETAILS	4-6
BRIDGE RAIL & TRANSITION DETAILS	7-8

LIST OF CT DOT STANDARD SHEETS

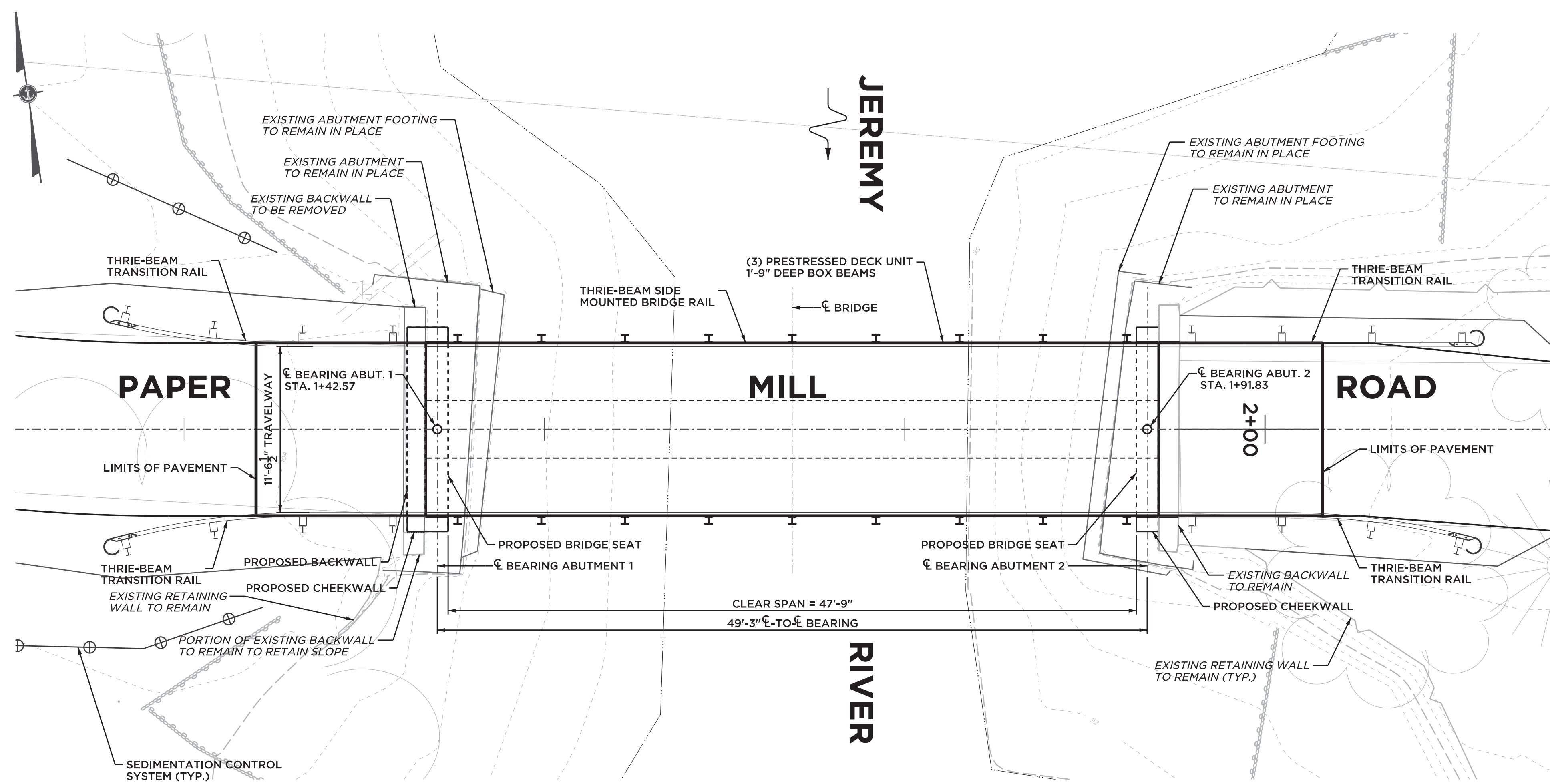
HW-822_01 TEMPORARY PRECAST CONCRETE BARRIER CURB
TR-1220_01 SIGNS FOR CONSTRUCTION AND PERMIT OPERATIONS
TR-1220_02 CONSTRUCTION SIGN SUPPORTS AND CHANNELIZING DEVICES

PREPARED BY:

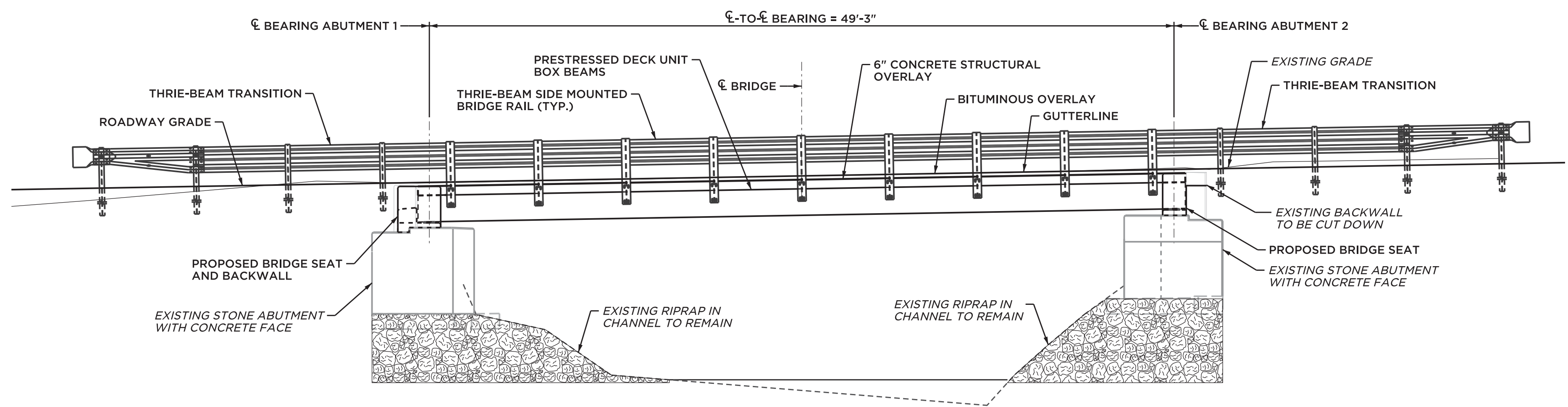
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BRIDGE LAYOUT
SCALE: 3/16" = 1'-0"



BRIDGE ELEVATION
SCALE: 3/16" = 1'-0"

GENERAL NOTES

CONSTRUCTION SPECIFICATIONS: CONNECTICUT DEPARTMENT OF TRANSPORTATION FORM 817 (2016), WITH SUPPLEMENTAL SPECIFICATIONS DATED JULY 2019 AND SPECIAL PROVISIONS.

DESIGN SPECIFICATIONS: AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS EIGHTH EDITION (AASHTO 2018) WITH THE INTERIM SPECIAL PROVISIONS UP TO AND INCLUDING 2019, AS SUPPLEMENTED BY THE CONNECTICUT DEPARTMENT OF TRANSPORTATION BRIDGE DESIGN MANUAL (2003), LATEST REVISION.

DESIGN STRESSES: CLASS "A" CONCRETE BASED ON $f_c = 3,000$ PSI
CLASS "F" CONCRETE BASED ON $f_c = 4,000$ PSI
PRECAST CONCRETE $f_c = 6,500$ PSI (MIN.)
REINFORCING BARS $f_y = 60,000$ PSI
WELDED WIRE FABRIC $f_y = 65,000$ PSI

CONCRETE STRENGTH: THE SPECIFIED CONCRETE STRENGTH USED IN DESIGN, F'C, OF THE CONCRETE COMPONENTS IS NOTED ABOVE. THE MINIMUM COMPRESSIVE STRENGTH OF THE CONCRETE IN THE CONSTRUCTED COMPONENTS SHALL CONFORM TO THE REQUIREMENTS OF "SECTION 6.01 CONCRETE FOR STRUCTURES."

LIVE LOAD: HL-93

FUTURE PAVING ALLOWANCE: NONE

CLASS "A" CONCRETE: CLASS "A" CONCRETE SHALL BE USED FOR THE ENTIRE SUBSTRUCTURE.

CLASS "F" CONCRETE: CLASS "F" CONCRETE SHALL BE USED FOR STRUCTURAL OVERLAY

EXPOSED EDGES: EXPOSED EDGES OF CONCRETE SHALL BE BEVELED 1" X 1" UNLESS DIMENSIONED OTHERWISE.

REINFORCEMENT: ALL REINFORCEMENT SHALL BE ASTM A615 GRADE 60.

CONCRETE COVER: ALL REINFORCEMENT SHALL HAVE 2" COVER UNLESS DIMENSIONED OTHERWISE.

GALVANIZED REINFORCING BARS: ALL NON-PRESTRESSED STEEL, IN PRESTRESSED CONCRETE MEMBERS, INCLUDING REINFORCEMENT EXTENDING OUT OF THE UNITS, AND CAST-IN-PLACE REINFORCED MEMBERS, SHALL BE GALVANIZED BAR REINFORCEMENT.

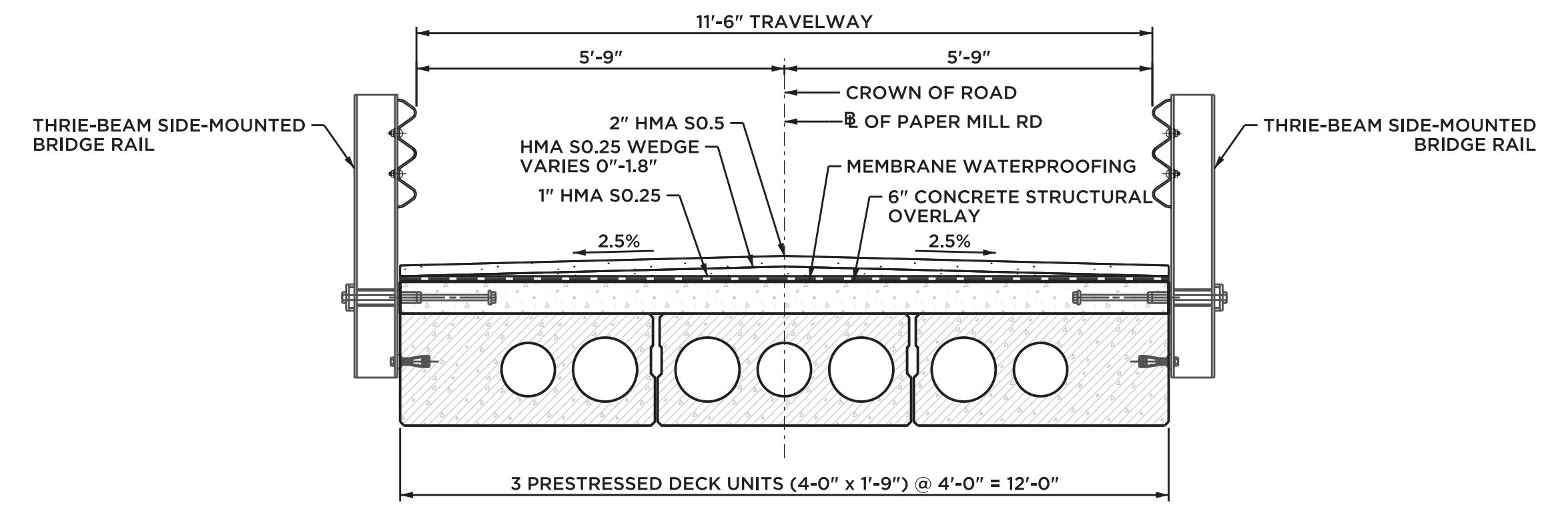
JOINT SEAL: ALL NON-PRESTRESSED STEEL SHALL BE GALVANIZED AFTER FABRICATION TO THE REQUIREMENTS OF ASTM A767, CLASS 1, INCLUDING SUPPLEMENTAL REQUIREMENTS.

CONSTRUCTION JOINTS: JOINT SEAL SHALL CONFIRM TO THE CONNECTICUT DEPARTMENT OF TRANSPORTATION FORM 817 (2016), WITH SUPPLEMENTAL SPECIFICATIONS DATED JULY 2017.

EXISTING DIMENSIONS: CONSTRUCTION JOINTS, OTHER THAN THOSE SHOWN ON THE PLANS, WILL NOT BE PERMITTED WITHOUT THE PRIOR APPROVAL OF THE ENGINEER.

DIMENSIONS: DIMENSIONS OF THE EXISTING STRUCTURE SHOWN ON THESE PLANS ARE FOR GENERAL REFERENCE ONLY. THEY ARE NOT GUARANTEED. THE CONTRACTOR SHALL TAKE ALL FIELD MEASUREMENTS NECESSARY TO ASSURE PROPER FIT OF THE FINISHED WORK AND SHALL ASSUME FULL RESPONSIBILITY FOR THEIR ACCURACY. WHEN SHOP DRAWINGS BASED ON FIELD MEASUREMENTS ARE SUBMITTED FOR APPROVAL, THE FIELD MEASUREMENTS SHALL ALSO BE SUBMITTED FOR REFERENCE BY THE REVIEWER.

WHEN DECIMAL DIMENSIONS ARE GIVEN TO LESS THAN THREE DECIMAL PLACES, THE OMITTED DIGITS SHALL BE ASSUMED TO BE ZEROS.



BRIDGE SECTION
SCALE: 1/2" = 1'-0"

CONCRETE DISTRIBUTION

SUPERSTRUCTURE	C.Y.	46
SUBSTRUCTURE	C.Y.	10
TOTAL	C.Y.	56

DISCLAIMER

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS AND IS IN NO WAY WARRANTED TO INDICATE THE TRUE CONDITIONS OR ACTUAL QUANTITIES OR DISTRIBUTION OF QUANTITIES OF WORK WHICH WILL BE REQUIRED.

SHIPPING DATA

MEMBER	SHIPPING LENGTH	SHIPPING HEIGHT	SHIPPING WIDTH	SHIPPING WEIGHT
B1	50'-9"	1'-9"	4'-0"	45 KIPS
B2	50'-9"	1'-9"	4'-0"	39.5 KIPS
B3	50'-9"	1'-9"	4'-0"	45 KIPS

NOTICE TO BRIDGE INSPECTOR

THE CONNECTICUT DOT'S BRIDGE SAFETY PROCEDURES REQUIRE THIS BRIDGE TO BE INSPECTED FOR, BUT NOT LIMITED TO, ALL APPROPRIATE COMPONENTS INDICATED IN THE GOVERNING MANUALS FOR BRIDGE INSPECTION. ATTENTION MUST BE GIVEN TO INSPECTING THE FOLLOWING SPECIAL COMPONENTS AND DETAILS. (THE LISTING FOR COMPONENTS FOR SPECIFIC ATTENTION SHALL NOT BE CONSTRUED TO REDUCE THE IMPORTANCE OF INSPECTION OF ANY OTHER COMPONENT OF THE STRUCTURE). THE FREQUENCY OF INSPECTION OF THIS STRUCTURE SHALL BE IN ACCORDANCE WITH THE GOVERNING MANUALS FOR BRIDGE INSPECTION, UNLESS OTHERWISE DIRECTED BY CONNECTICUT DOT'S MANAGER OF BRIDGE SAFETY AND EVALUATION.

COMPONENT OR DETAIL	STRUCTURE SHEET REFERENCE

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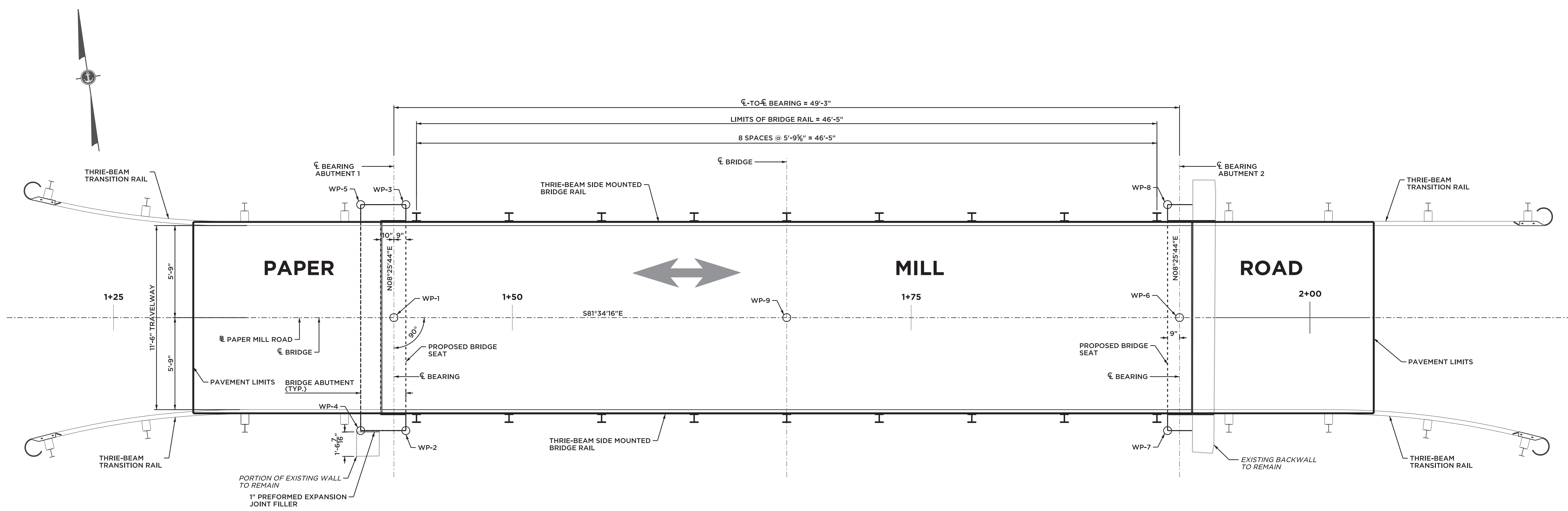
PROJ. ENGINEER: KBF
PROJ. MANAGER: MMZ
OFFICE REVIEW: SMM

REVISIONS

REHABILITATION OF BRIDGE NO. 05528
PREPARED FOR
TOWN OF COLCHESTER
GENERAL PLAN
PAPER MILL ROAD COLCHESTER, CT

PROJECT	DATE	SHEET NO.	OF
084-13	02/11/20	2	8

SCALE: AS NOTED




BRIDGE LAYOUT

WORKING POINTS AND COORDINATES					
WP NO.	DESCRIPTION	NORTHING	EASTING	STATION	OFFSET
1	BASELINE & CENTERLINE OF BEARING ABUTMENT 1	772453.36	1095538.89	1+42.58	0.00
2	SOUTHWEST OUTSIDE CORNER OF CHEEKWALL @ FRONT OF BRIDGE SEAT	772446.25	1095538.60	1+43.33	7.08
3	NORTHWEST OUTSIDE CORNER OF CHEEKWALL @ FRONT OF BRIDGE SEAT	772460.26	1095540.67	1+43.33	7.08
4	SOUTHWEST OUTSIDE CORNER OF CHEEKWALL @ BACK OF ABUTMENT	772446.66	1095535.79	1+40.49	7.08
5	NORTHWEST OUTSIDE CORNER OF CHEEKWALL @ BACK OF ABUTMENT	772460.68	1095537.87	1+40.49	7.08
6	BASELINE & CENTERLINE OF BEARING ABUTMENT 2	772446.14	1095587.61	1+91.83	0.00
7	SOUTHEAST OUTSIDE CORNER OF CHEEKWALL @ FRONT OF ABUTMENT	772439.25	1095585.83	1+91.08	7.08
8	NORTHEAST OUTSIDE CORNER OF CHEEKWALL @ FRONT OF ABUTMENT	772453.26	1095587.91	1+91.08	7.08
9	CENTERLINE OF BASELINE @ CENTERLINE OF BRIDGE	772449.75	1095563.25	1+67.20	0.00

FINISHED ELEVATIONS (AT TOP OF WEARING SURFACE)						
STATION	DESCRIPTION	LEFT GUTTER LINE		℄	RIGHT GUTTER LINE	
		ELEVATION	OFFSET	ELEVATION	ELEVATION	OFFSET
1+42.58	CENTERLINE ABUTMENT 1	104.72	6.00	104.87	104.72	6.00
1+67.20	CENTERLINE BRIDGE	105.15	6.00	105.30	105.15	6.00
1+91.83	CENTERLINE ABUTMENT 2	105.58	6.00	105.73	105.58	6.00

FINISHED ELEVATIONS (AT TOP OF STRUCTURAL SLAB)						
STATION	DESCRIPTION	LEFT GUTTER LINE		℄	RIGHT GUTTER LINE	
		ELEVATION	OFFSET	ELEVATION	ELEVATION	OFFSET
1+42.58	CENTERLINE ABUTMENT 1	104.47	6.00	104.47	104.47	6.00
1+67.20	CENTERLINE BRIDGE	104.90	6.00	104.90	104.90	6.00
1+91.83	CENTERLINE ABUTMENT 2	105.33	6.00	105.33	105.33	6.00

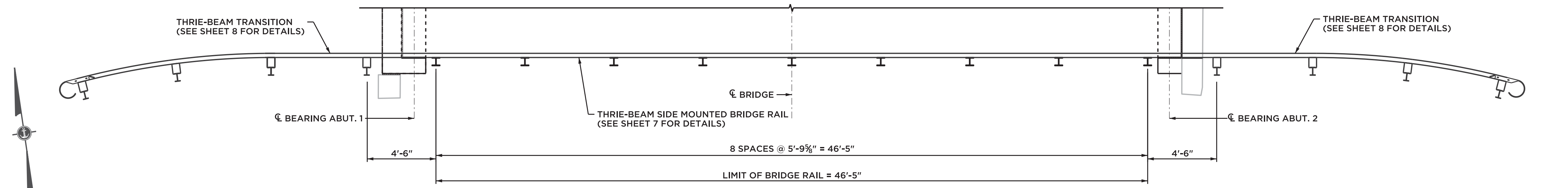


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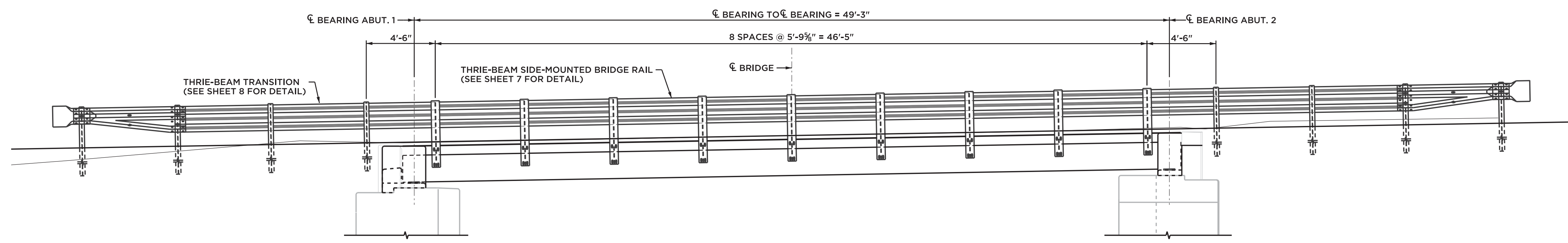
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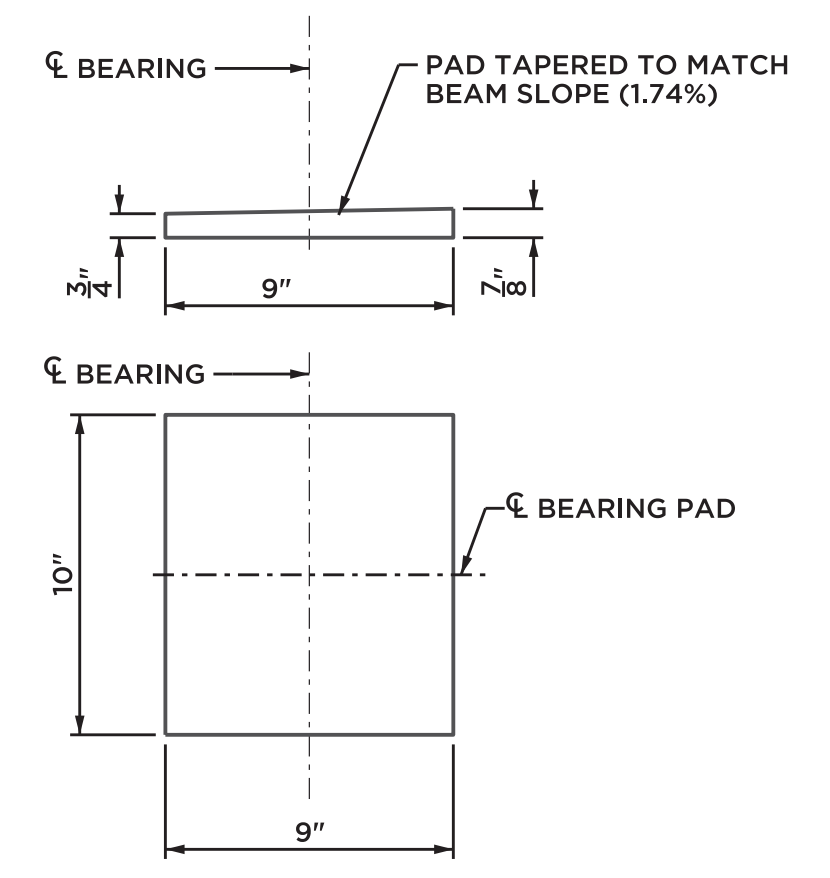
PROJ. ENGINEER	SMM	REHABILITATION OF BRIDGE NO. 05528 PREPARED FOR TOWN OF COLCHESTER LAYOUT PLAN
PROJ. MANAGER	MMZ	
OFFICE REVIEW	KBF	
REVISIONS		
PROJECT: PAPER MILL ROAD		
SCALE: 1" = 3'-0"	PROJECT: 084-13	DATE: 02/11/20
		SHEET NO. 3 OF 8



PLAN

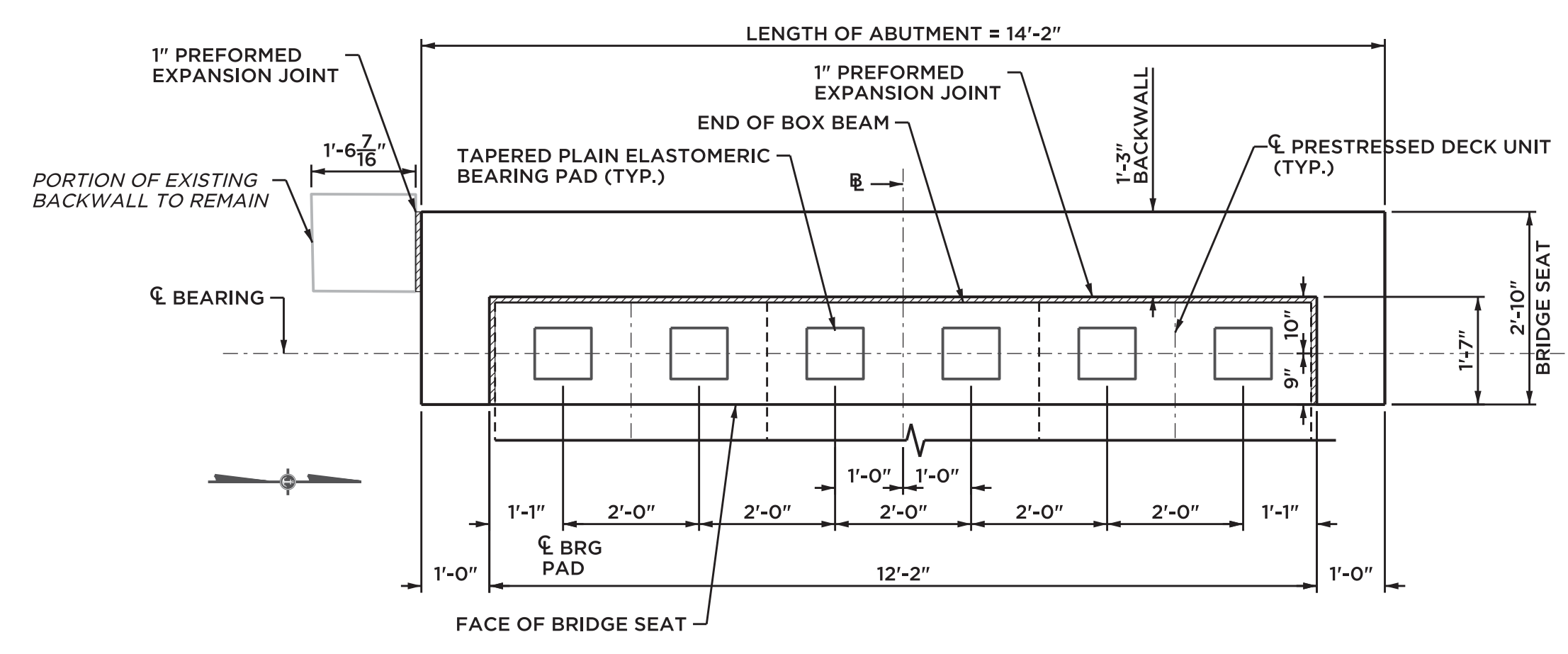


**ELEVATION
BRIDGE RAIL DETAIL**
SCALE: 1/4" = 1'-0"

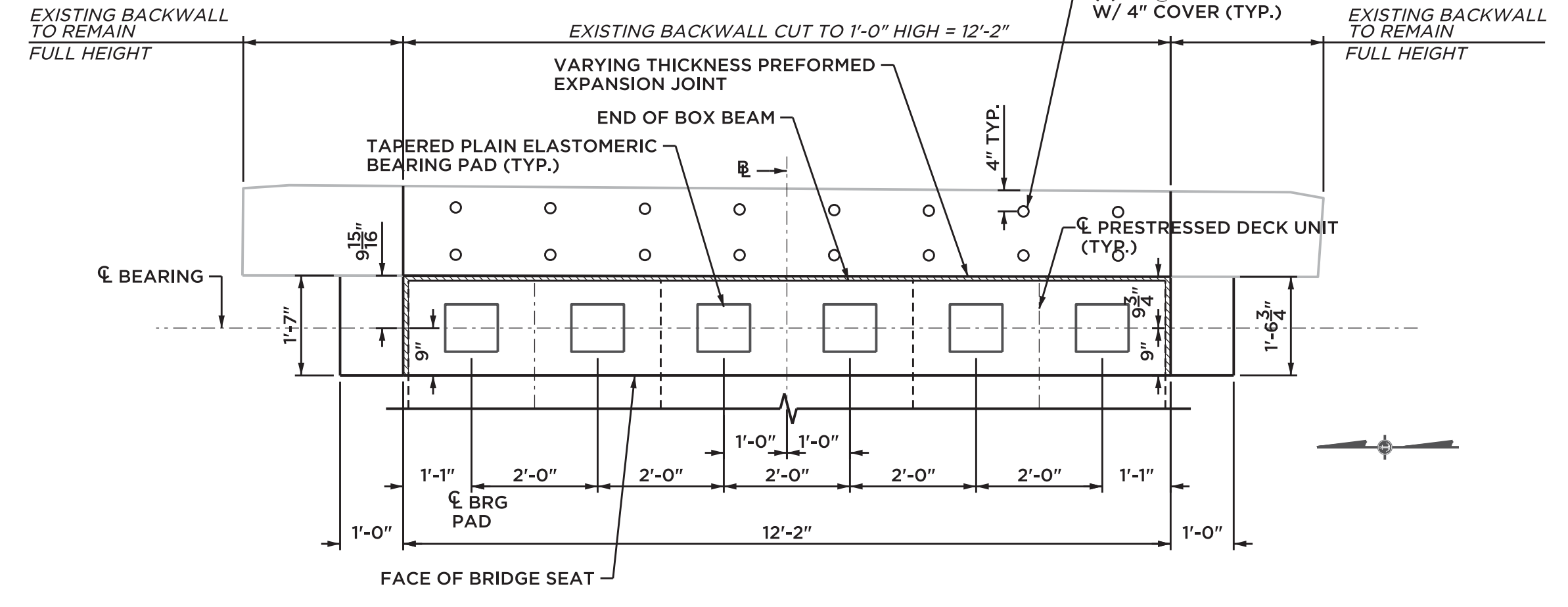


**TAPERED PLAIN
ELASTOMERIC BEARINGS DETAIL**
SCALE: 3" = 1'-0"

- BEARING ASSEMBLY NOTES:**
- THE ELASTOMER SHALL BE TYPE CR, GRADE 3 AS DEFINED BY ASTM D4014 AND SHALL HAVE A SHORE A DUROMETER HARDNESS OF 60+/-5 POINTS AND A SHEAR MODULUS WITHIN THE LIMITS OF 130 TO 175 PSI.
 - THE MAXIMUM DESIGN LOAD (DL+LL W/O IMPACT) FOR ELASTOMERIC BEARING IS 40.2 KIPS. THIS INFORMATION IS PROVIDED FOR THE PROOF LOAD TEST DESCRIBED IN THE SPECIAL PROVISION.
 - THE ELASTOMERIC BEARINGS SHALL BE INSTALLED WHEN THE AMBIENT AIR TEMPERATURE IS BETWEEN 41°F AND 77°F HAS BEEN WITHIN THIS RANGE FOR MORE THAN TWO HOURS.
 - THE CONCRETE ABUTMENT SEAT SHALL BE CAREFULLY FINISHED SMOOTH TO AN EVEN, LEVEL SURFACE AND SHALL SHOW NO VARIATIONS FROM A TRUE PLANE GREATER THAN 1/16".
 - BEARING DESIGNED IN ACCORDANCE WITH AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS 2017, SECTION 14, METHOD A.



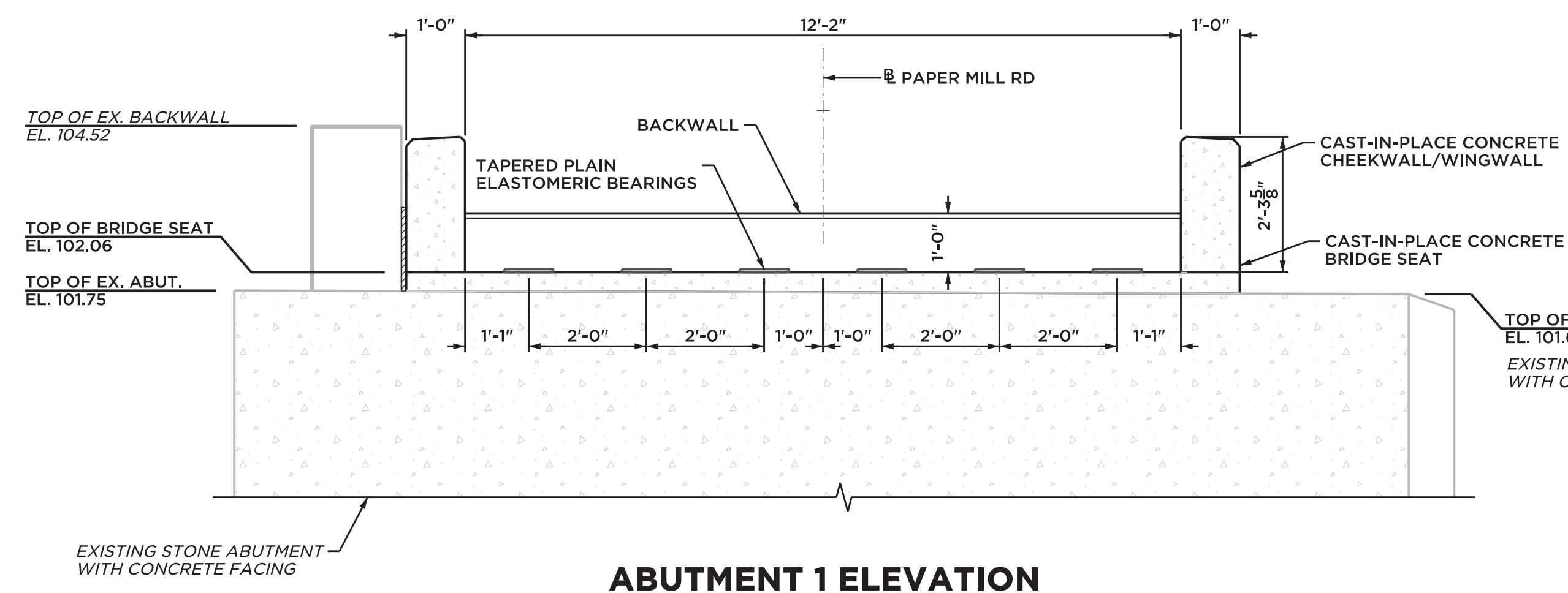
ABUTMENT 1 LAYOUT
SCALE: 1/2" = 1'-0"



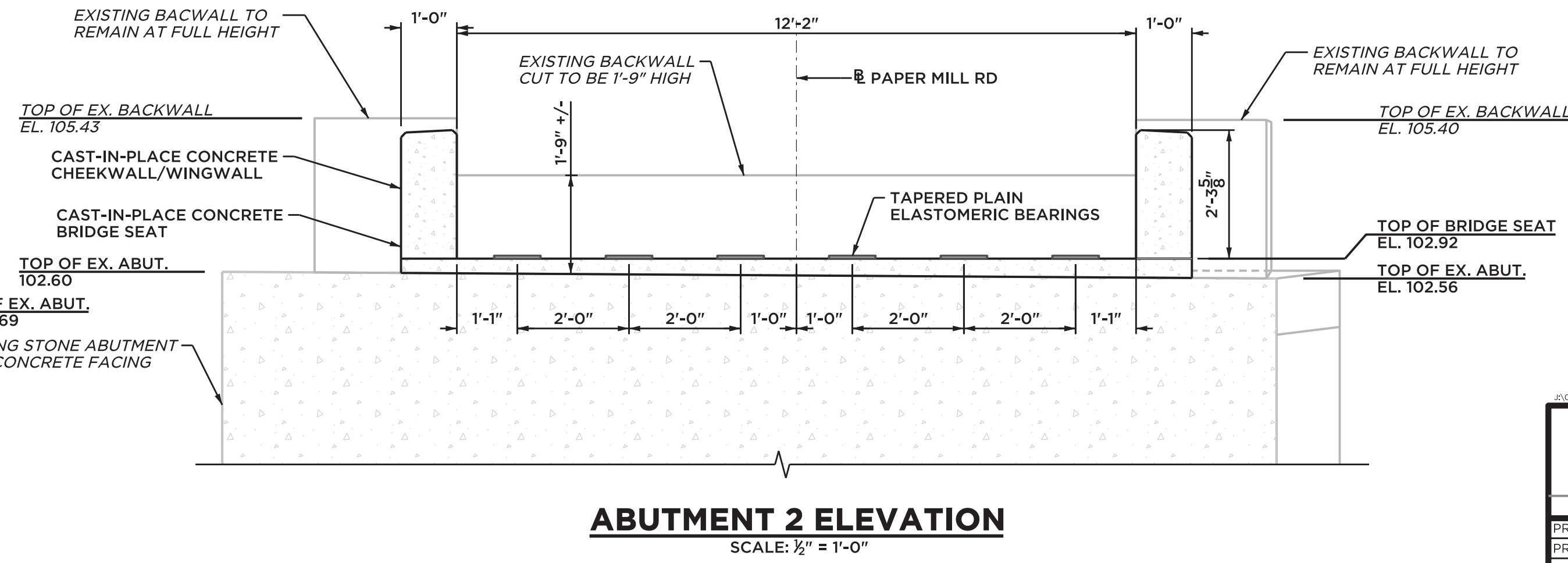
ABUTMENT 2 LAYOUT
SCALE: 1/2" = 1'-0"

ELASTOMERIC BEARING TABLE					
BEARING REACTIONS					
LOCATION	TYPE	MAX. DESIGN REACTION (KIPS)	DL* (KIPS)	LL W/O IMPACT* (KIPS)	TOTAL ROTATION (RADIAN)
ALL	PEP	40.2	18	22.2	0.00576

* SERVICE LOADS



ABUTMENT 1 ELEVATION
SCALE: 1/2" = 1'-0"



ABUTMENT 2 ELEVATION
SCALE: 1/2" = 1'-0"

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OFFICE REVIEW KBF

REVISIONS

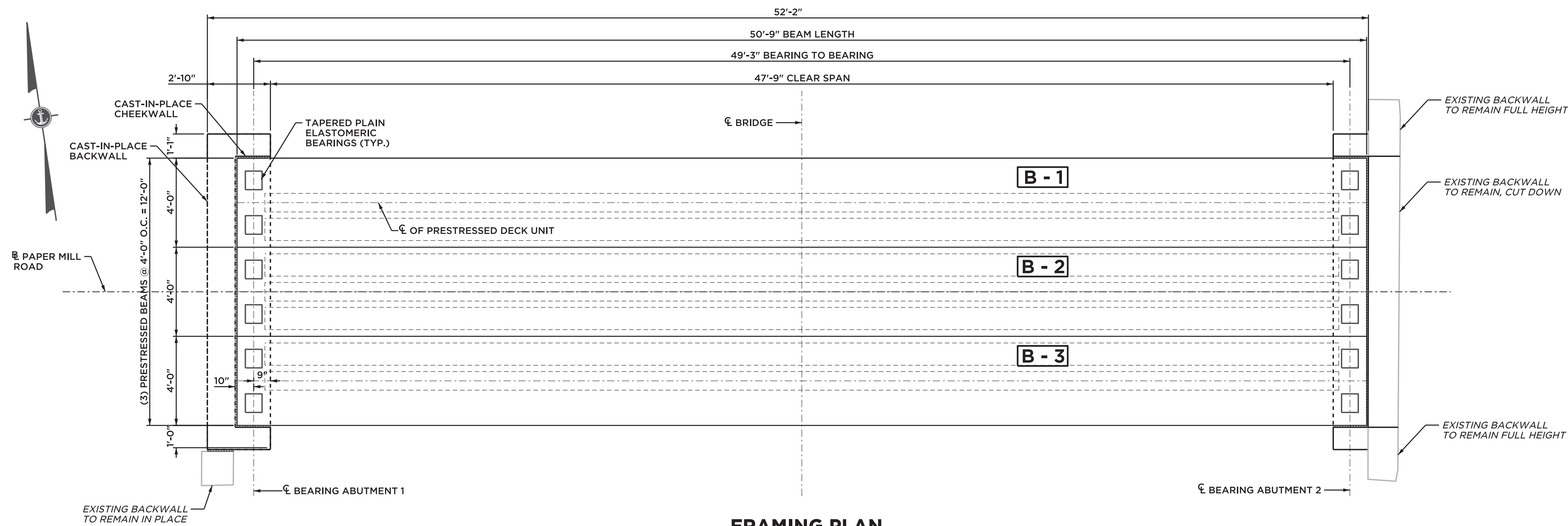
PREPARED FOR
TOWN OF COLCHESTER

ABUT. PLAN & ELEV. & RAIL DETAILS

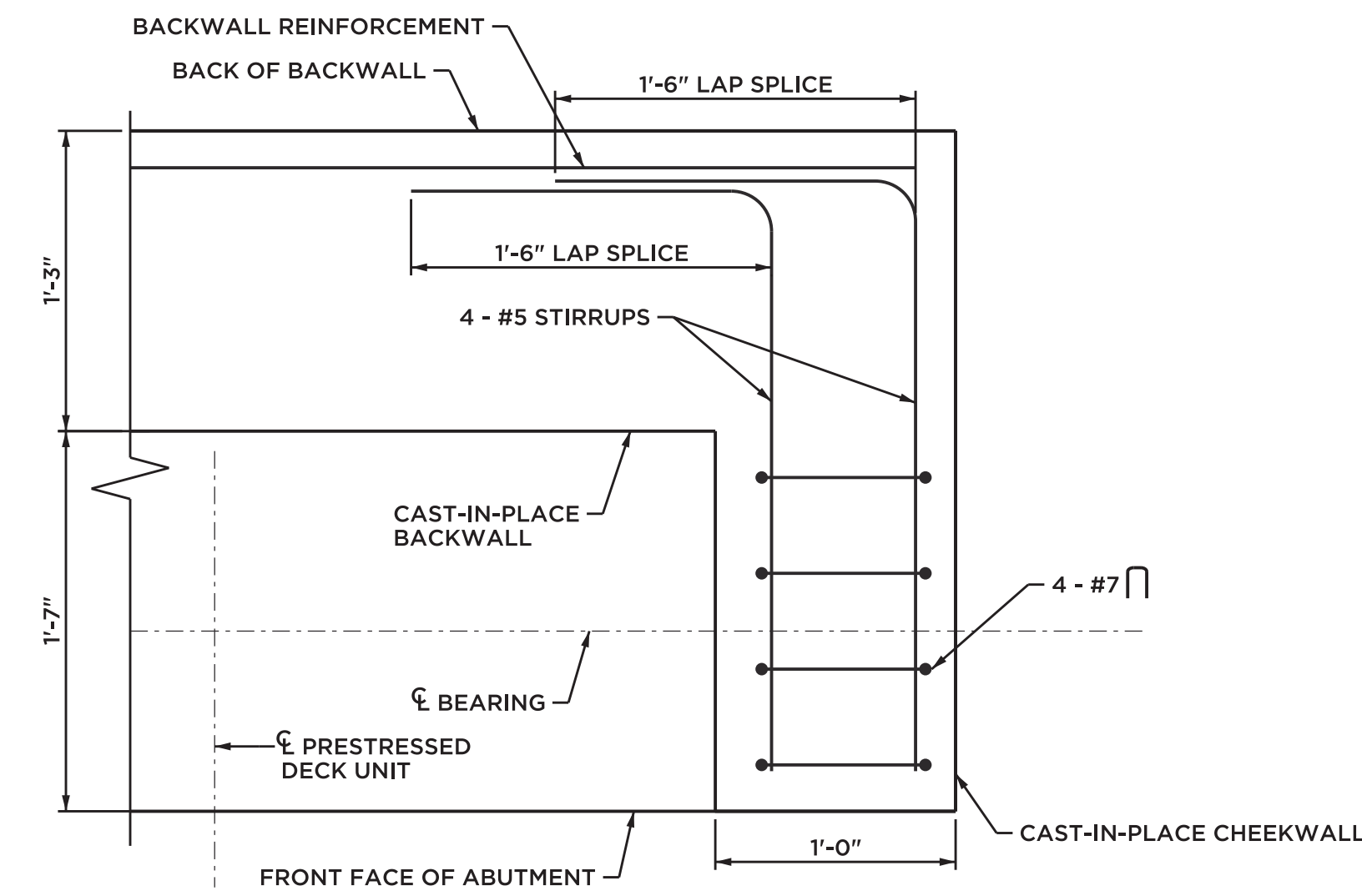
PAPER MILL ROAD COLCHESTER, CT

PROJECT	DATE	SHEET NO.	4	OF	8
084-13	02/11/20				

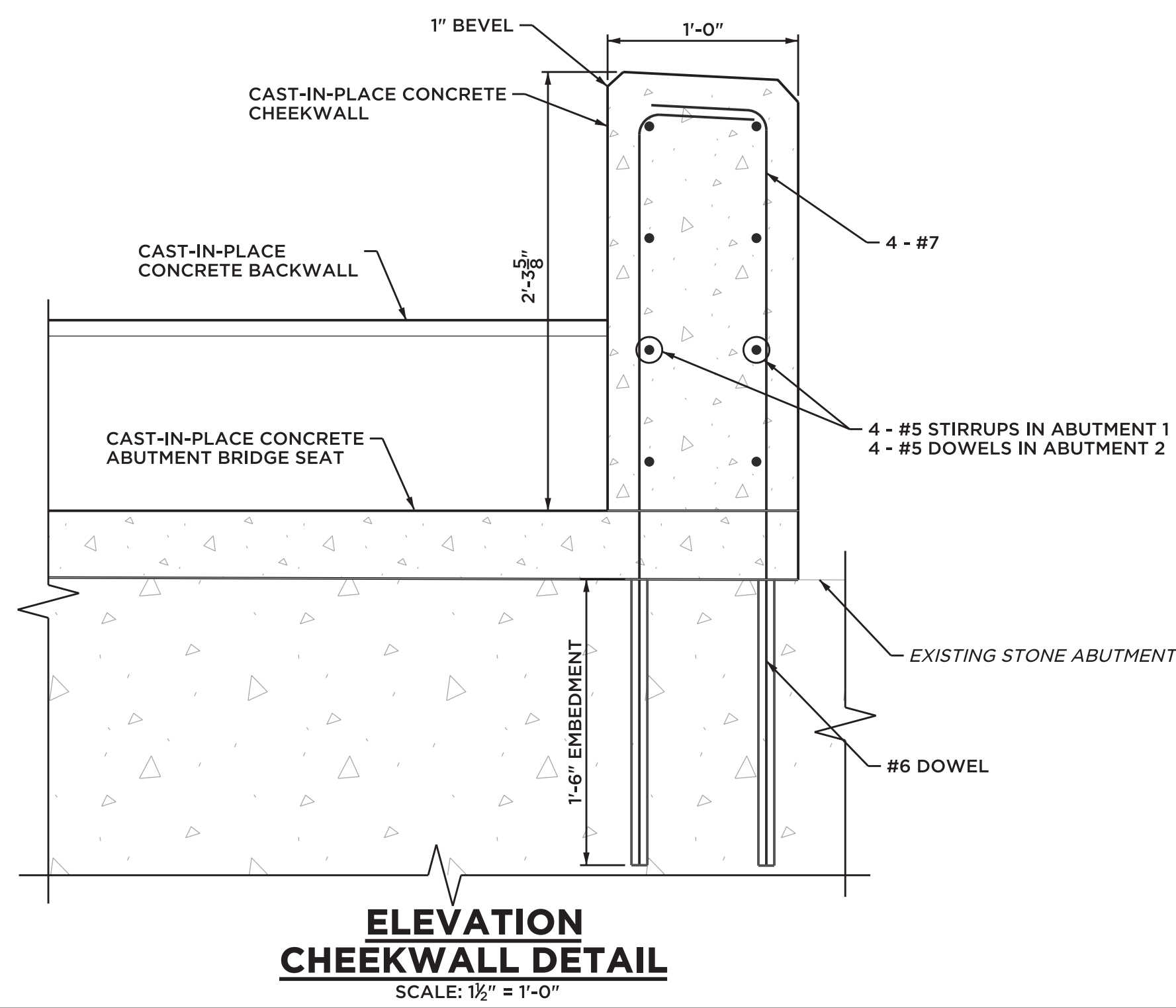
SCALE: AS NOTED



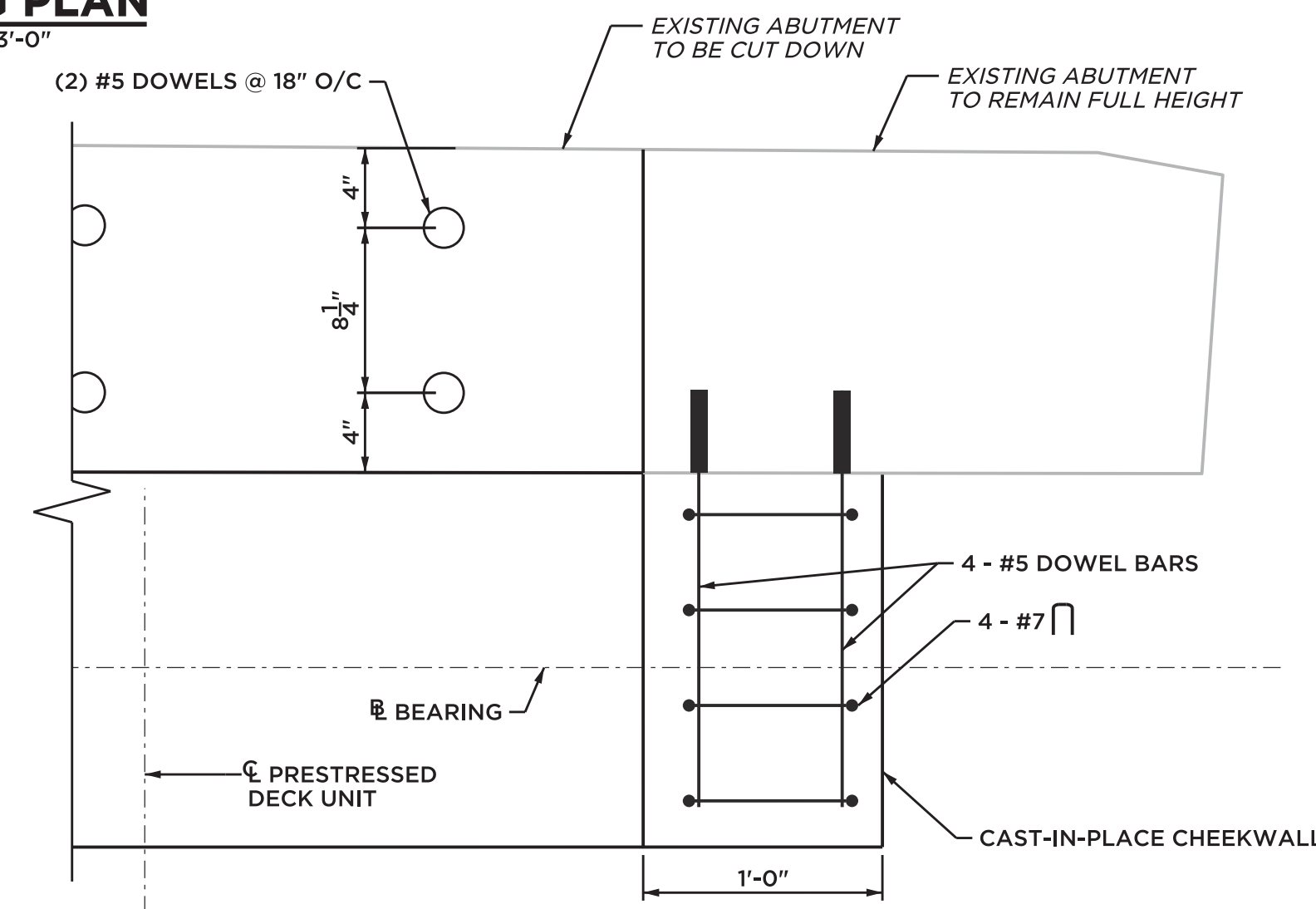
FRAMING PLAN
SCALE: 1" = 3'-0"



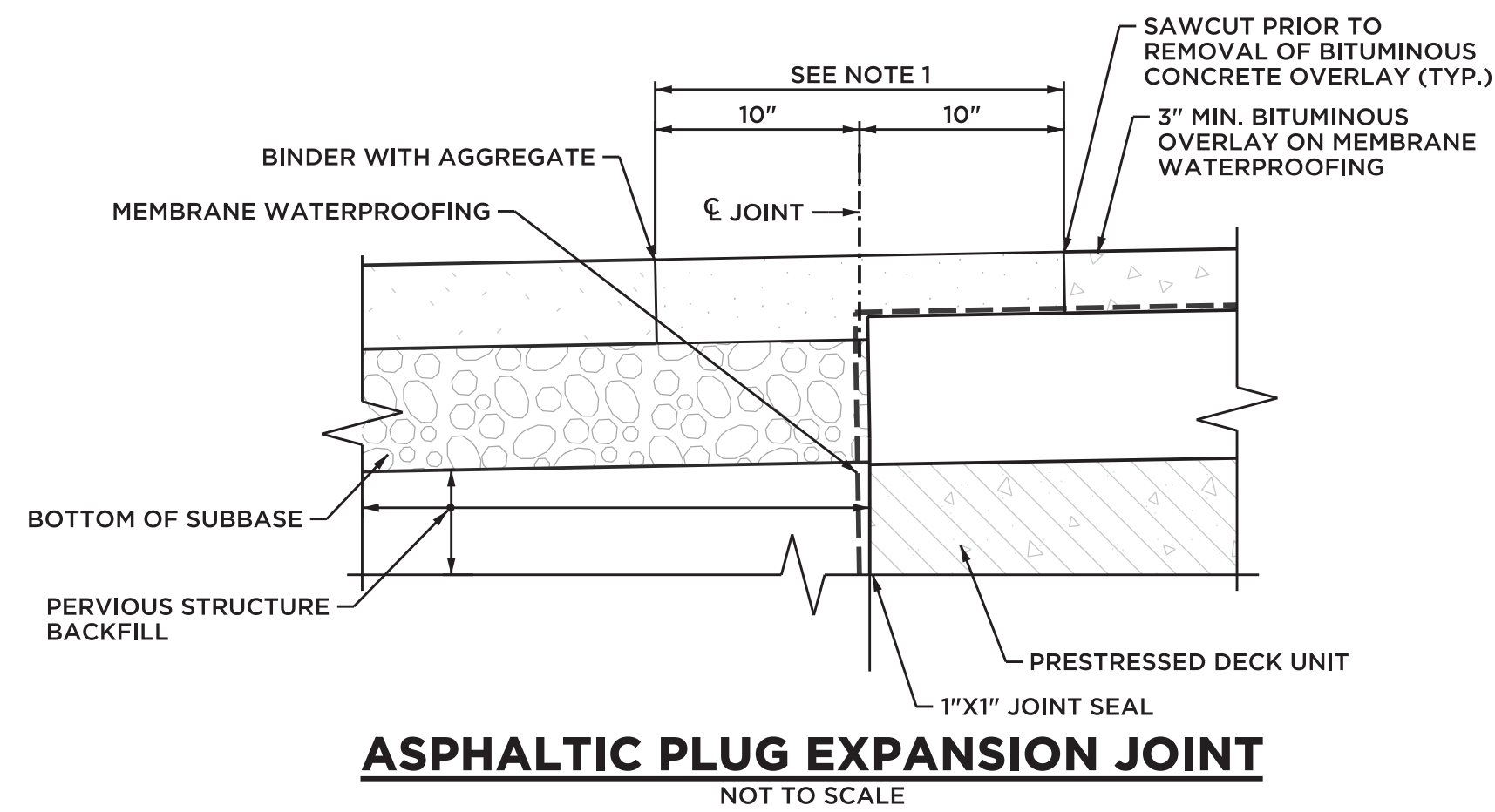
CHEEKWALL PLAN ABUTMENT 1
SCALE: 1 1/2" = 1'-0"



ELEVATION CHEEKWALL DETAIL
SCALE: 1 1/2" = 1'-0"

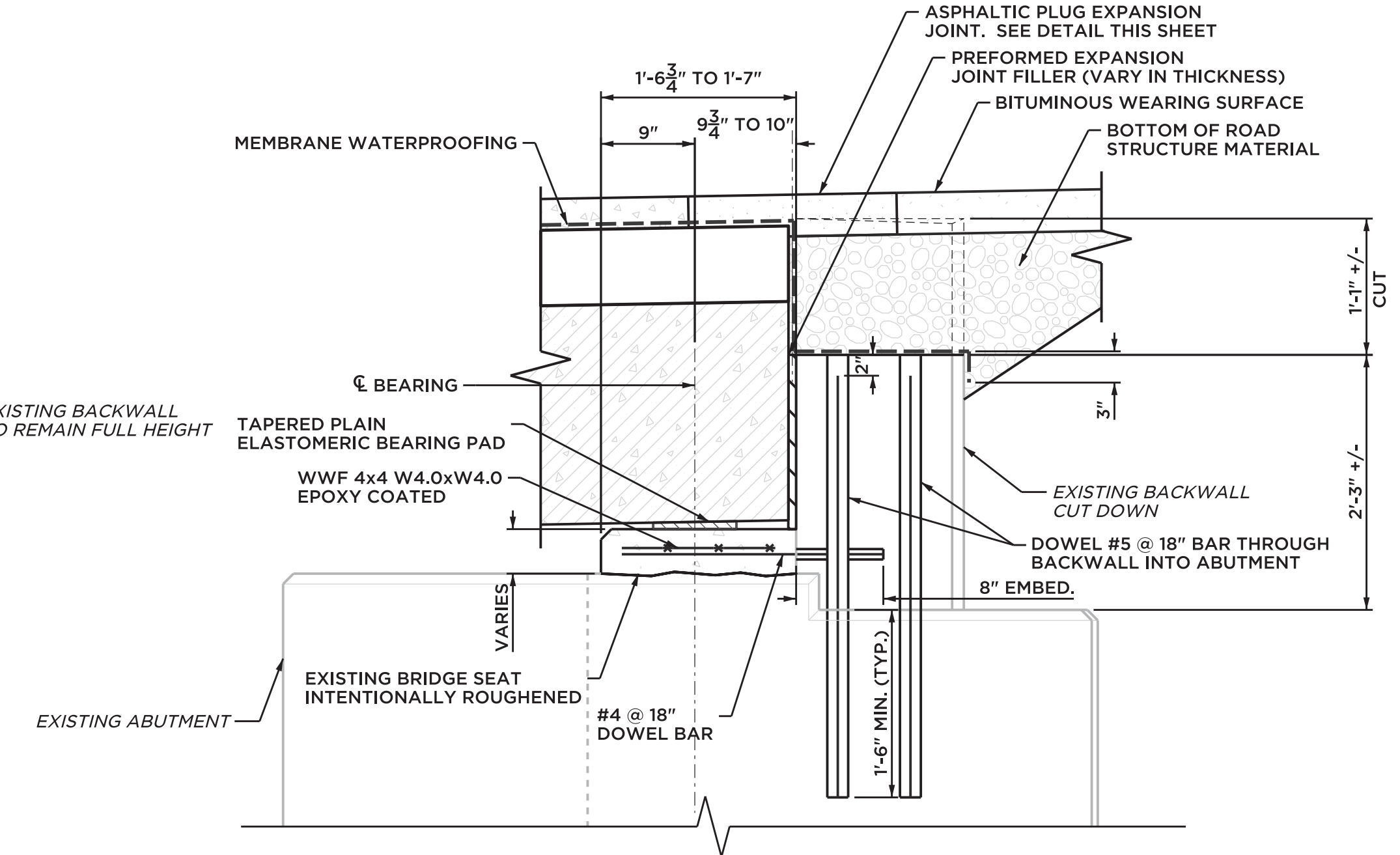


CHEEKWALL PLAN ABUTMENT 2
SCALE: 1 1/2" = 1'-0"

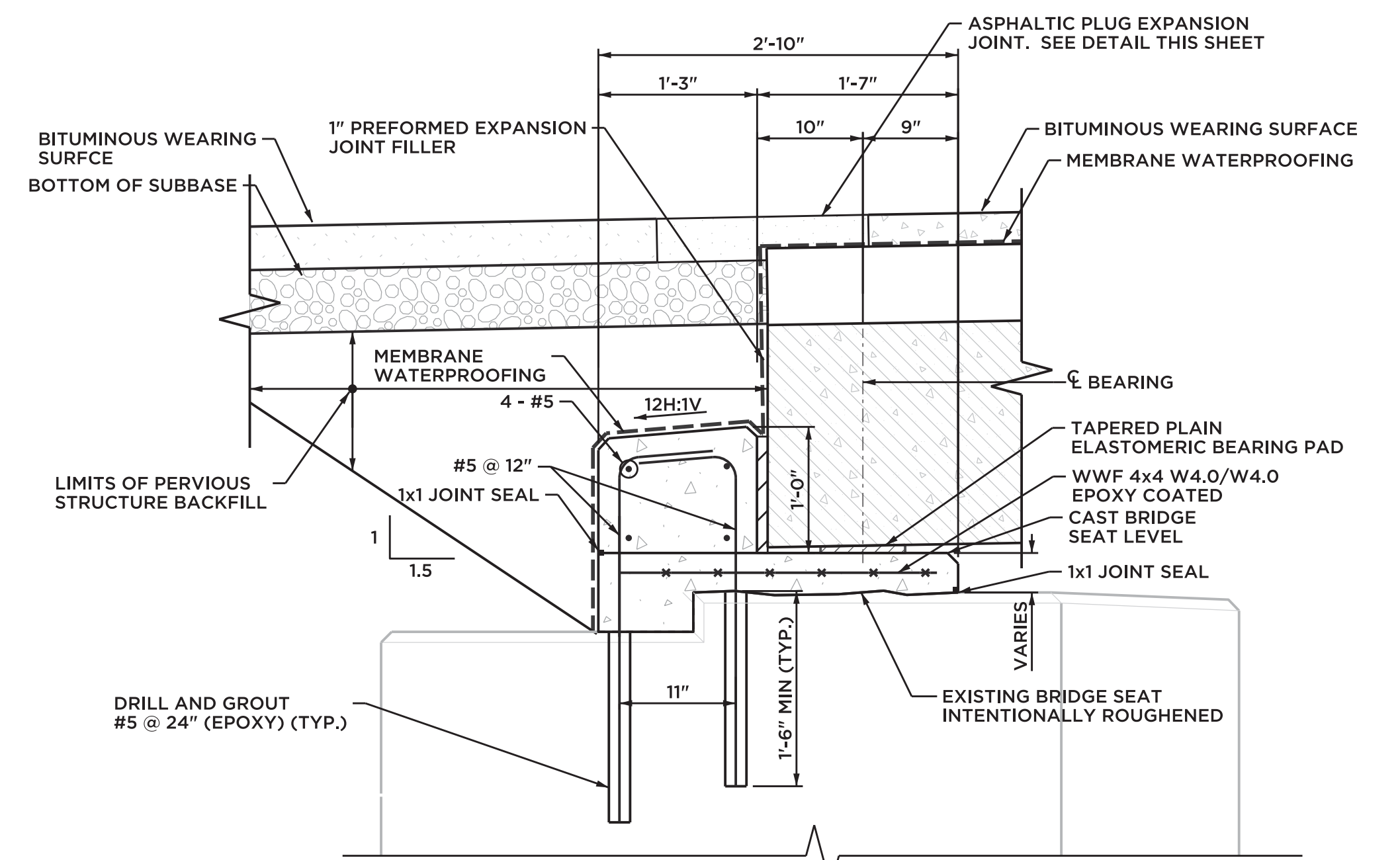


ASPHALTIC PLUG EXPANSION JOINT
NOT TO SCALE

NOTE: REMOVE NEW BITUMINOUS CONCRETE OVERLAY AND MEMBRANE WATERPROOFING. REPLACE WITH ASPHALTIC EXPANSION JOINT SYSTEM. TO BE PAID FOR UNDER THE ITEM "ASPHALTIC PLUG EXPANSION JOINT SYSTEM".

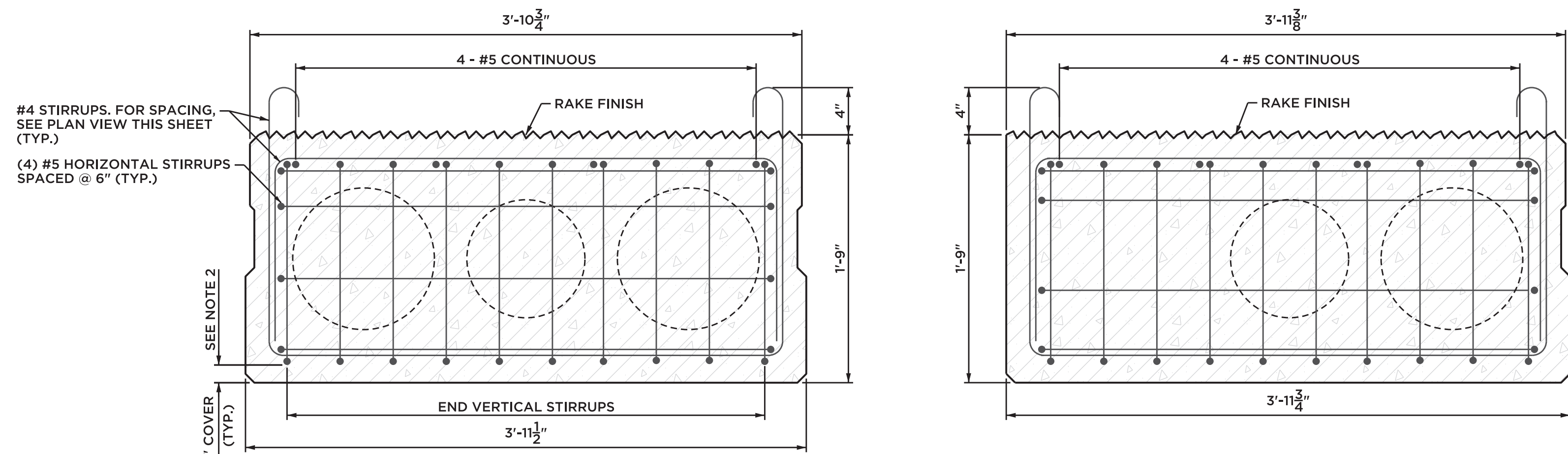


BRIDGE SEAT @ ABUTMENT 2
SCALE: 1 1/2" = 1'-0"



BRIDGE SEAT @ ABUTMENT 1
SCALE: 1 1/2" = 1'-0"

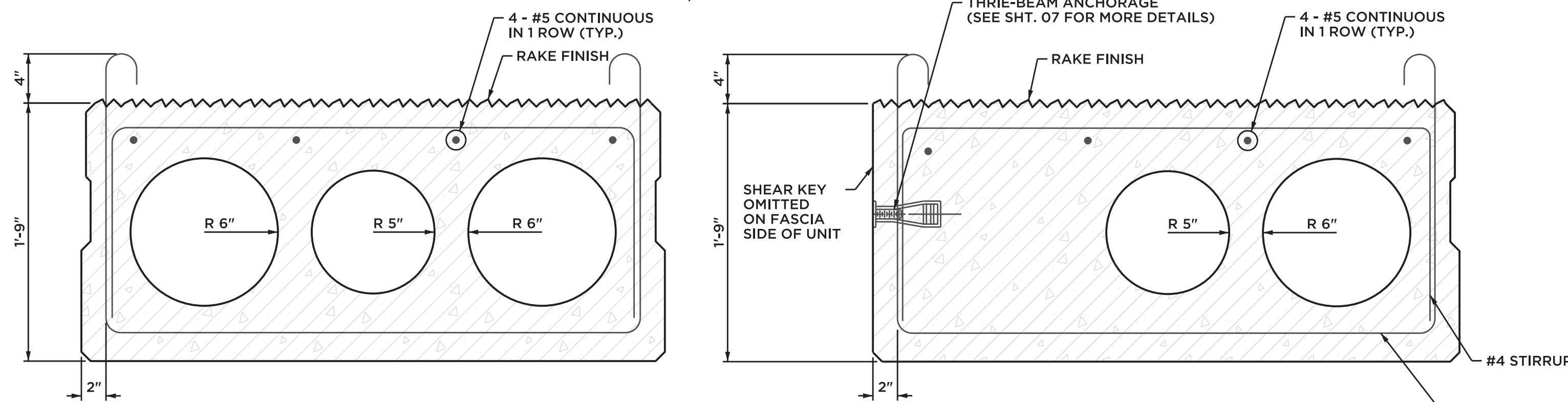
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PROJ. ENGINEER	SMM	REHABILITATION OF BRIDGE NO. 05528 PREPARED FOR TOWN OF COLCHESTER FRAMING PLAN & ABUTMENT DETAILS PAPER MILL ROAD COLCHESTER, CT	
PROJ. MANAGER	MMZ		
OFFICE REVIEW	KBF		
REVISIONS		PROJECT	DATE
		084-13	02/11/20
		SHEET NO.	5 OF 8
SCALE: AS NOTED			



END VIEW DECK UNIT B2

END VIEW DECK UNITS B1 & B3

END REINFORCING
SCALE: 1-1/2" = 1'-0"



SECTION @ MIDSPAN DECK UNITS B2

SECTION @ MIDSPAN DECK UNITS B1 & B3

SCALE: 1-1/2" = 1'-0"

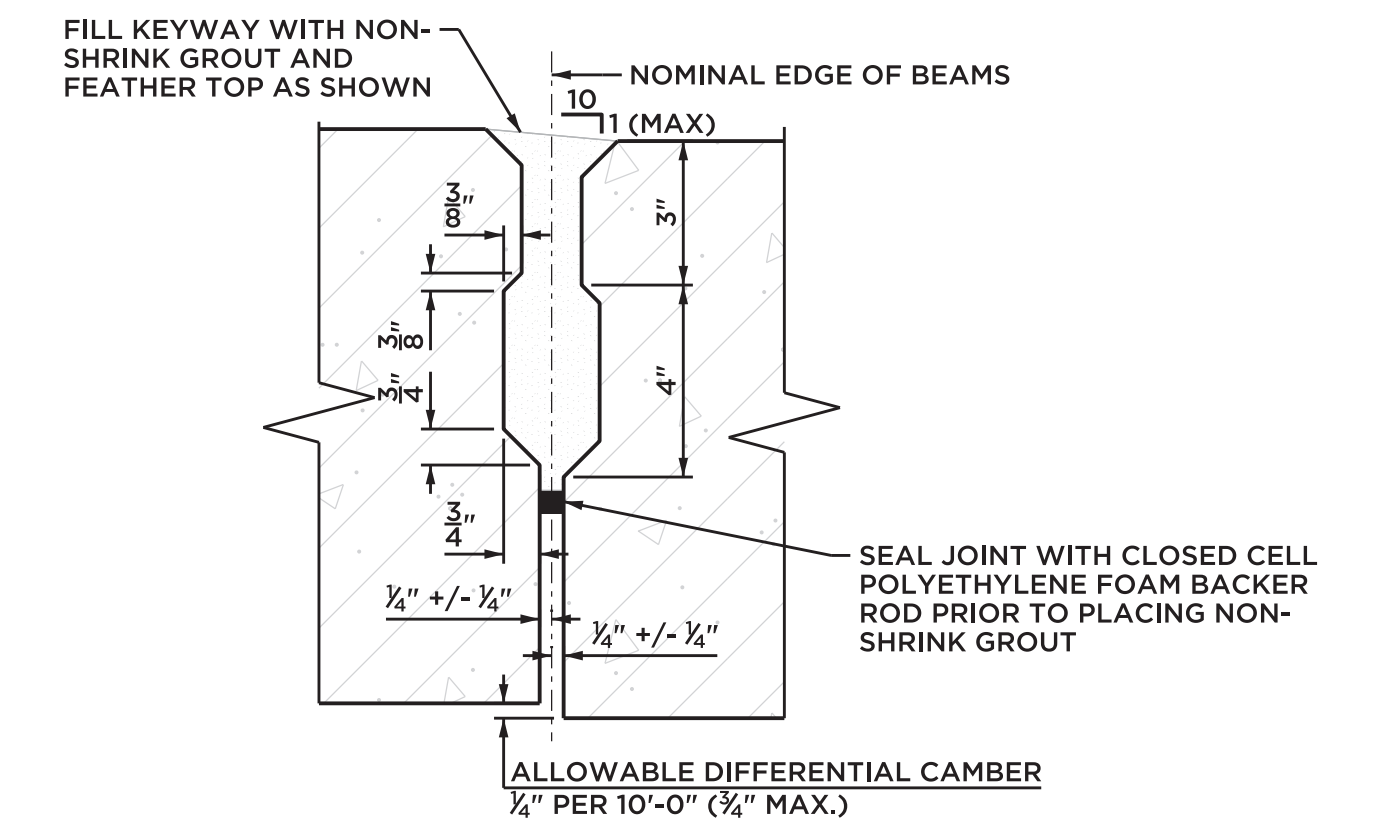
SCALE: 1-1/2" = 1'-0"

REINFORCEMENT NOTES:

- SPLAY STIRRUPS TO AVOID CONFLICTS WITH TRANSVERSE TIE STRAND HOLES.
- THIS COVER IS REQUIRED TO FACILITATE THE PLACEMENT OF THE BOTTOM PRESTRESSING STRANDS. THE FABRICATOR MAY MODIFY COVER (1/2" MIN.) IF NO CONFLICTS EXIST WITH THE PRESTRESSING STRANDS.

MEMBER NUMBER	NUMBER OF STRANS	C.G. OF STRANDS	
		END (A)	MIDSPAN (A)
B1	26	5.818"	5.231"
B2	26	5.538"	6.182"
B3	26	5.818"	5.231"

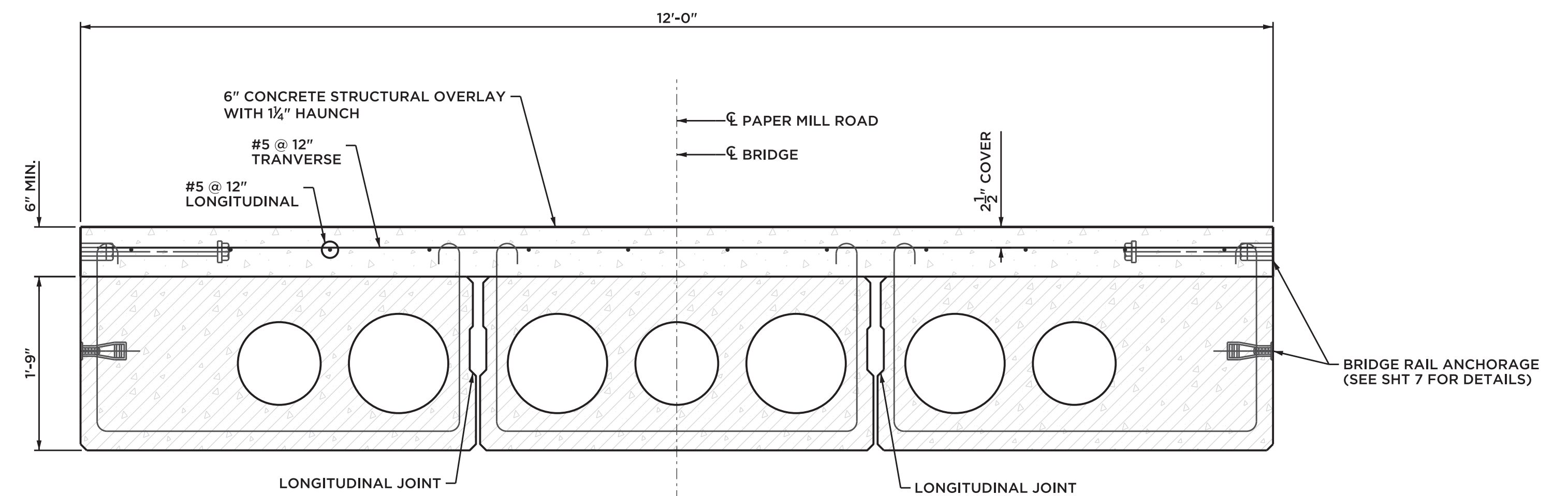
MEMBER NUMBER	DEBOND LENGTH FROM END OF BEAM	NUMBER OF DEBONDED STRANDS



TYPICAL LONGITUDINAL JOINT
NOT TO SCALE

NOTES:

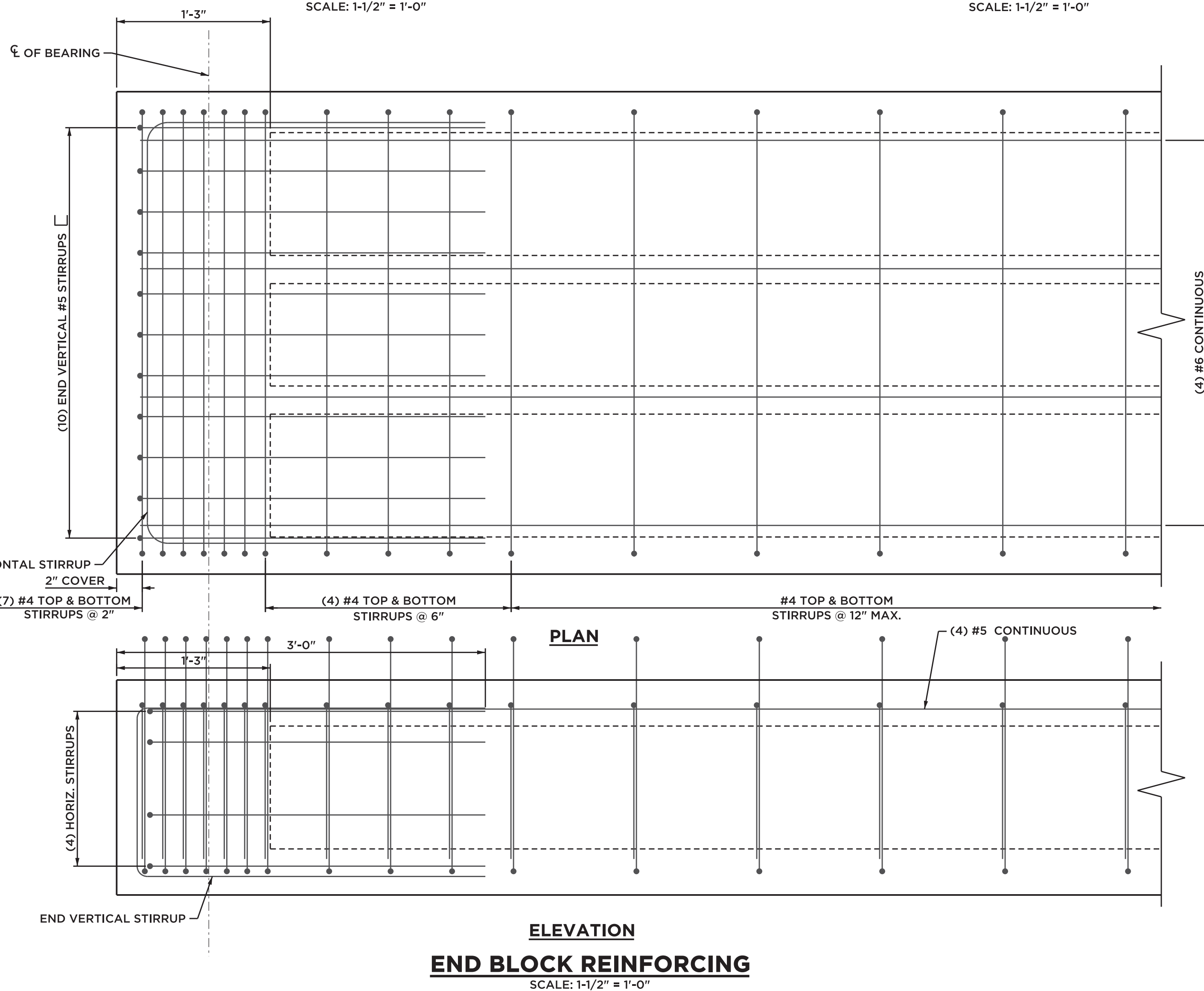
- THE DECK UNITS SHALL BE PLACED AT THE NOMINAL SPACING SHOWN ON THE PLAN WITH A GAP BETWEEN THE UNITS. THE WIDTH OF THE GAPS WILL VARY DUE TO THE SWEEP OF THE UNITS.
- GROUT FOR SHEAR KEYS SHALL BE RODDED OR VIBRATED TO ENSURE THAT ALL VOIDS IN THE SHEAR KEY ARE FILLED.



STRUCTURAL OVERLAY DETAIL
SCALE: 1" = 1'-0"

PRESTRESSED DECK UNITS NOTES

- CONCRETE DESIGN STRENGTH FOR PRECAST DECK UNITS SHALL CONFORM TO THE FOLLOWING REQUIREMENTS:
FINAL COMPRESSIVE STRENGTH $f_c = 6,500$ PSI
INITIAL COMPRESSIVE STRENGTH $f_{ci} = 5,500$ PSI
DECK COMPRESSIVE STRENGTH $f_c = 4,000$ PSI
- PRESTRESSING STRANDS SHALL BE 0.6" DIAMETER UNCOATED LOW RELAXATION STRANDS, AND SHALL CONFORM TO THE FOLLOWING REQUIREMENTS:
ULTIMATE TENSILE STRENGTH $F_s = 270,000$ PSI
JACKING FORCE PER STRAND (f_p) = 43,943 LBS
- PRESTRESSING STRANDS SHALL BE PLACED 2 INCHES (MINIMUM) ON CENTER AND SHALL HAVE A MINIMUM COVER OF 1 1/2".
- ENDS OF PRESTRESSED DECK UNITS SHALL BE VERTICAL AFTER APPLICATION OF FULL DEAD LOADS.
- DRILLING OF HOLES OR USE OF POWER ACTUATED TOOLS ON PRESTRESSED DECK UNITS WILL NOT BE PERMITTED.
- NO SUPERIMPOSED DEAD OR LIVE LOADS SHALL BE APPLIED TO THE BUTTED DECK UNITS UNTIL THE GROUT IN THE LONGITUDINAL SHEAR KEYS HAS REACHED A SEVEN-DAY COMPRESSIVE STRENGTH OF 4,500 PSI.
- ALL BAR REINFORCEMENT IN THE DECK UNITS SHALL BE GALVANIZED AFTER FABRICATION TO THE REQUIREMENTS OF ASTM A767, CLASS 1, INCLUDING SUPPLEMENTAL REQUIREMENTS.
- THE DE-BONDED STRANDS SHALL BE WELL DISTRIBUTED ACROSS THE MEMBER CROSS SECTION. NO TWO ADJACENT STRANDS (EITHER HORIZONTALLY OR VERTICALLY) SHALL BE DE-BONDED, ALTHOUGH DIAGONALLY ADJACENT STRANDS MAY BE DE-BONDED. THE OUTER MOST STRANDS OF EACH LAYER SHALL NOT BE DE-BONDED.



END BLOCK REINFORCING
SCALE: 1-1/2" = 1'-0"

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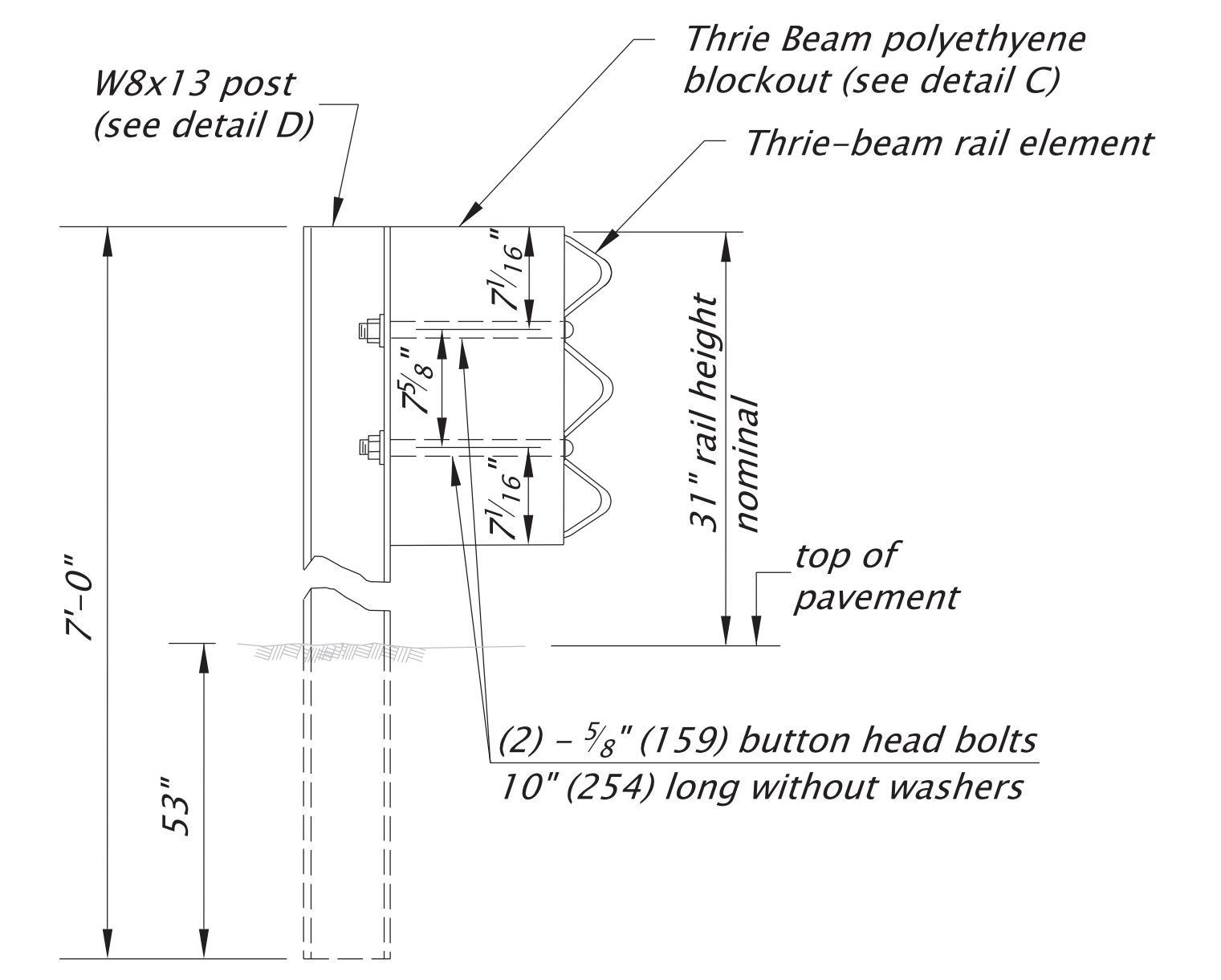
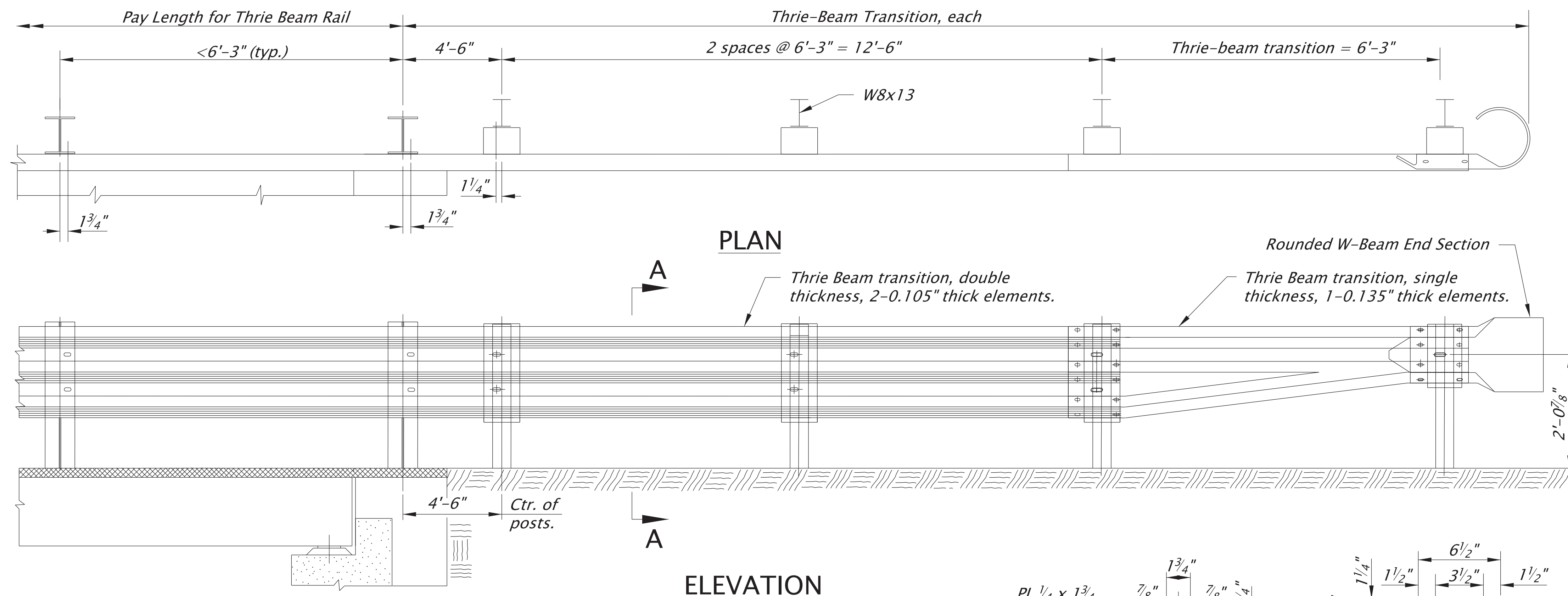
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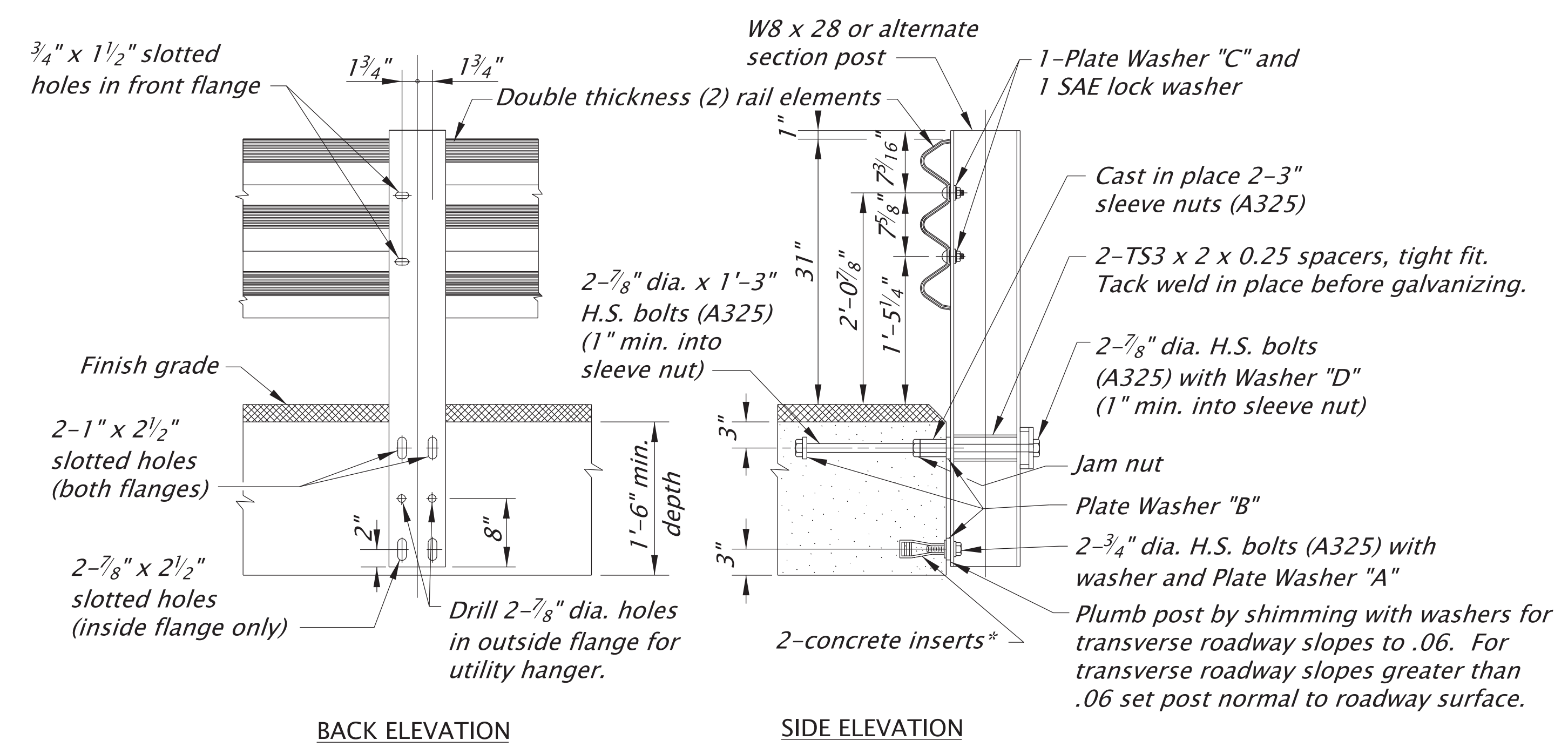
REVISIONS

REPLACEMENT OF BRIDGE NO. 05528
PREPARED FOR
TOWN OF COLCHESTER
PRESTRESSED DECK UNIT DETAILS
PAPER MILL ROAD COLCHESTER, CT

PROJECT 084-13 DATE 02/11/20 SHEET NO. 6 OF 8
SCALE: 1" = 1'-0"



**SECTION A-A
W8x13 THRIE-BEAM POST
CONNECTION DETAIL**
not to scale



POST DETAILS: SIDE MOUNT

NOTE
Field ream bolt holes in double thickness rail at splice locations.
Repair damaged coating according to Specifications.

***CONCRETE INSERTS**
Hot-dip galvanized expanded coil concrete inserts with closed-back ferrule threaded to receive 3/4" dia., Gr36 (ASTM A307)
Minimum insert length= 4 1/2"
Minimum safe working load in tension= 4000 lbs.

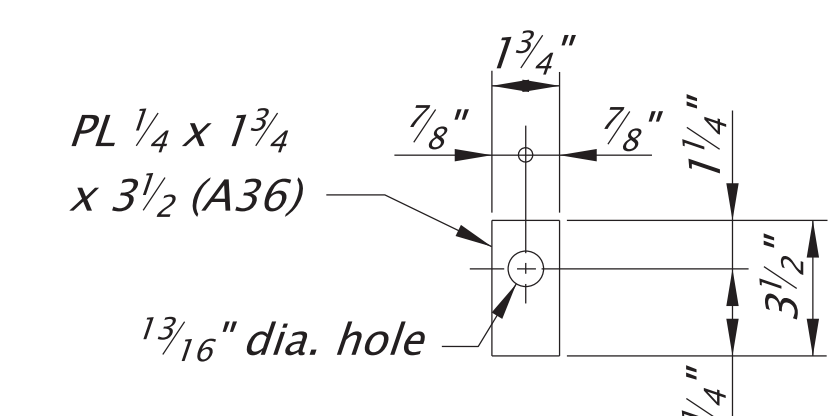


PLATE WASHER "A"
Position washer to completely cover slotted hole.

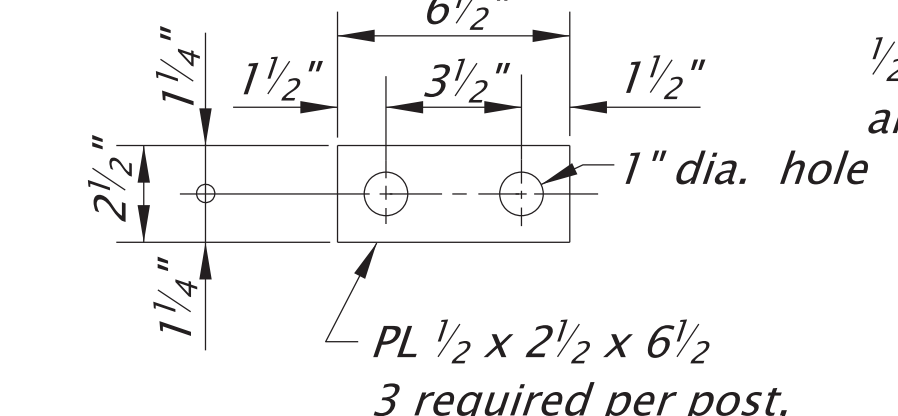


PLATE WASHER "B"

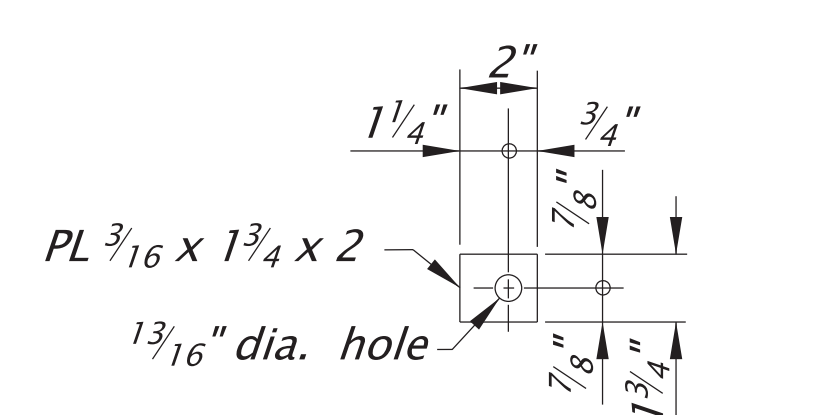
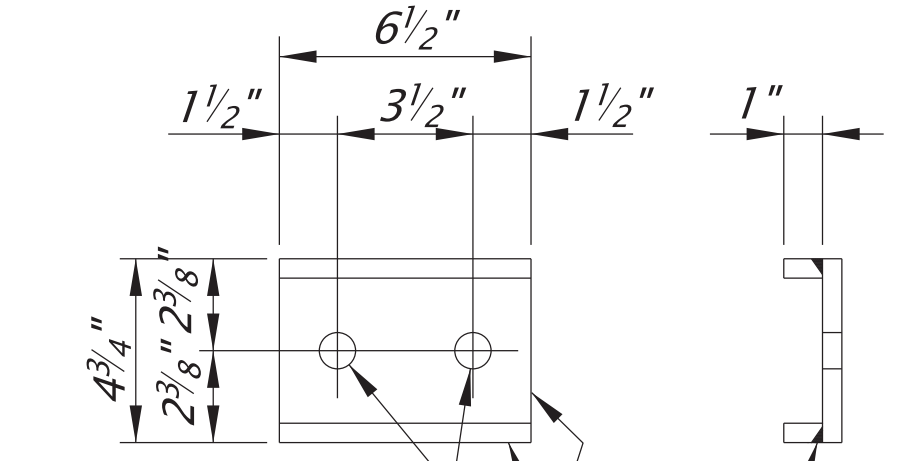
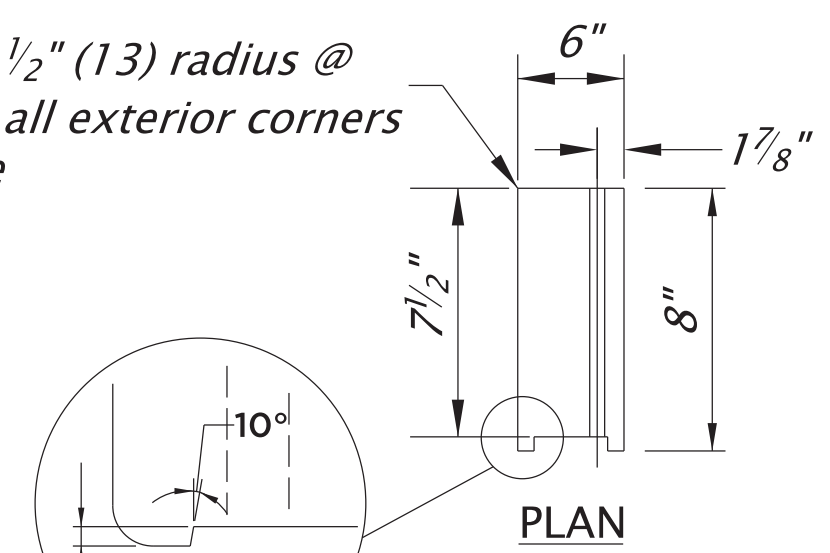


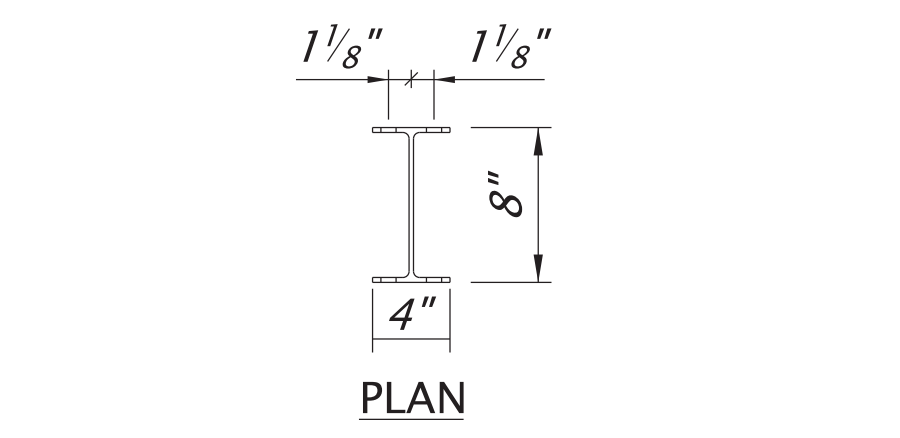
PLATE WASHER "C"
Position washer to completely cover slotted hole.



WASHER "D"



**DETAIL C
THRIE-BEAM
POLYETHYLENE BLOCKOUT**
not to scale

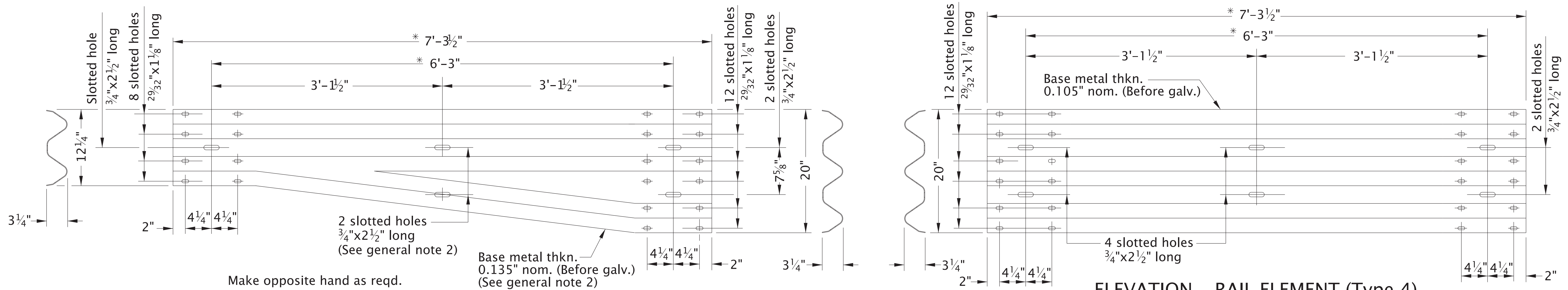


**DETAIL D
THRIE-BEAM POST DETAIL
W8x13 BOLT HOLE LOCATION**
not to scale

GENERAL NOTES

- Provide steel plates conforming to AASHTO Specification M183 (ASTM A36), unless noted otherwise.
- Provide anchor bolts conforming to ASTM A325. (AASHTO M164).
- Provide guardrail hardware as shown on Sheet 8 of 8.
- Hot dip galvanize all structural steel and hardware after fabrication.
- Fabricate railing to the horizontal and vertical alignment of the structure. Install posts normal to grade. When wearing surface thickness varies due to beam camber and/or superelevation, vary rail post lengths to provide uniform rail height.
- Tap nuts and inserts 0.0021 +0.01/-0.00 oversize after galvanizing in accordance with ASTM A563.
- Tighten upper high strength post bolts 1/6 turn past snug tight condition. Tighten lower high strength post bolts 1/3 turn past snug tight condition.

		41 Sequin Drive Glastonbury, CT 06033 Phone: (860) 633-9770 Fax: (860) 633-9971 www.anchorengr.com	
		Civil Engineering • Environmental Consulting • Land Surveying • Construction Management	
PROJ. ENGINEER: KBF PROJ. MANAGER: MMZ OFFICE REVIEW: SMM	REHABILITATION OF BRIDGE 05528 PREPARED FOR: TOWN OF COLCHESTER BRIDGE RAIL & TRANSITION 1 OF 2 PAPER MILL ROAD COLCHESTER, CT		
REVISIONS:	PROJECT: 084-13 DATE: 02/11/20	SHEET NO. 7 OF 8	SCALE: NOT TO SCALE



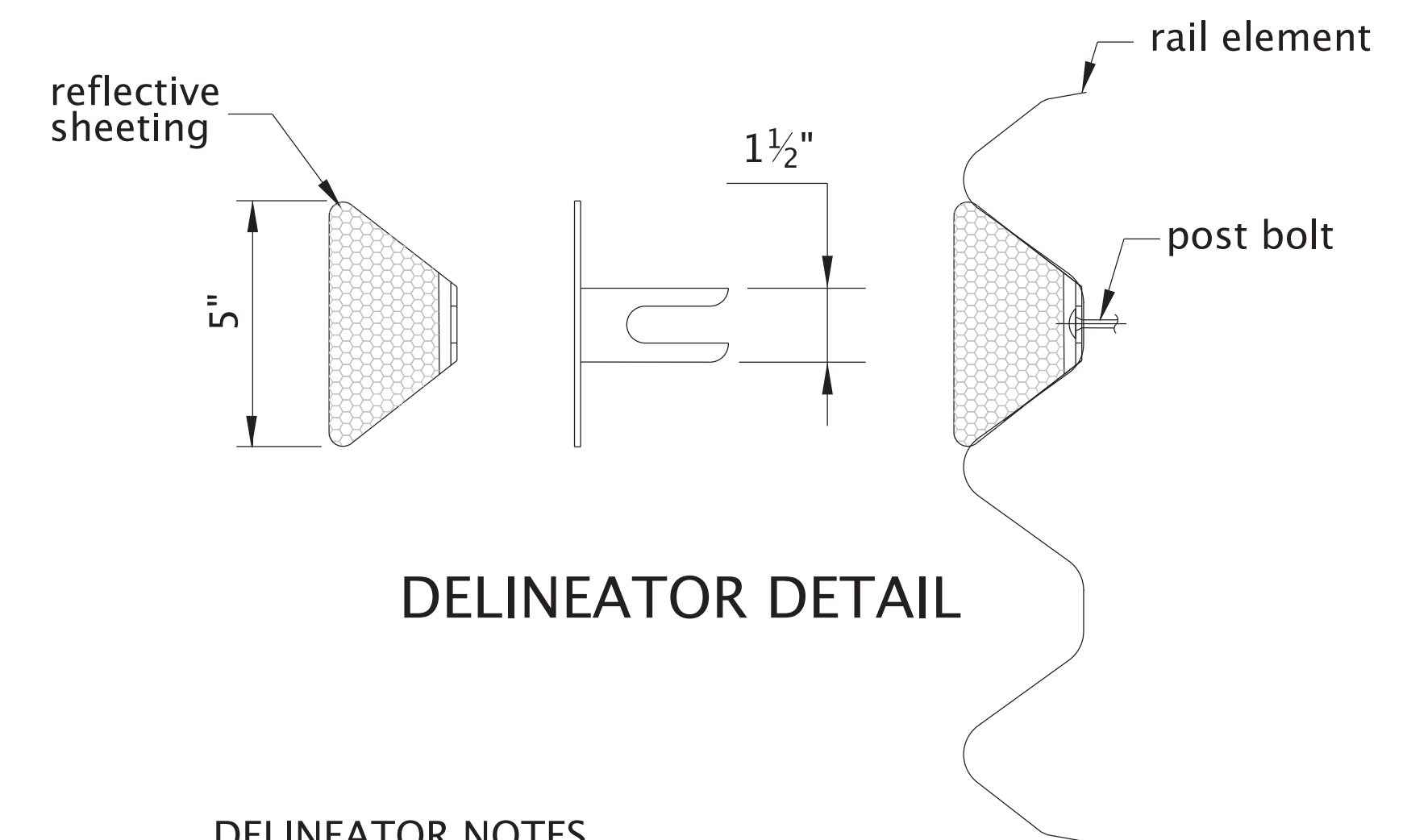
ELEVATION - ASYMMETRICAL TRANSITION ELEMENT

ELEVATION - RAIL ELEMENT (Type 4)

* See general note 2

GENERAL NOTES FOR ALL DETAILS:

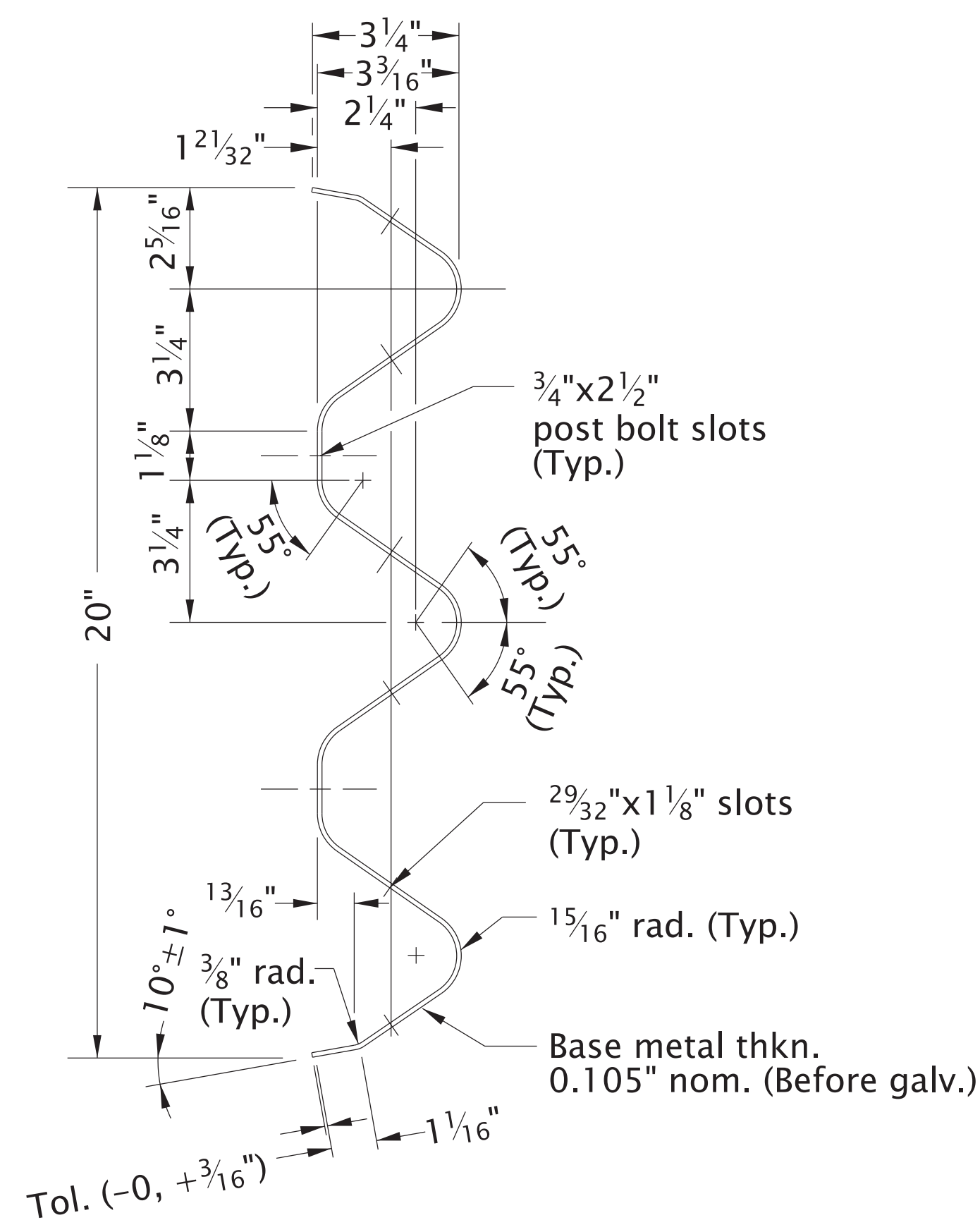
1. Lap guardrail in direction of adjacent traffic.
2. Hole layout per manufacturer with appropriate post and block.



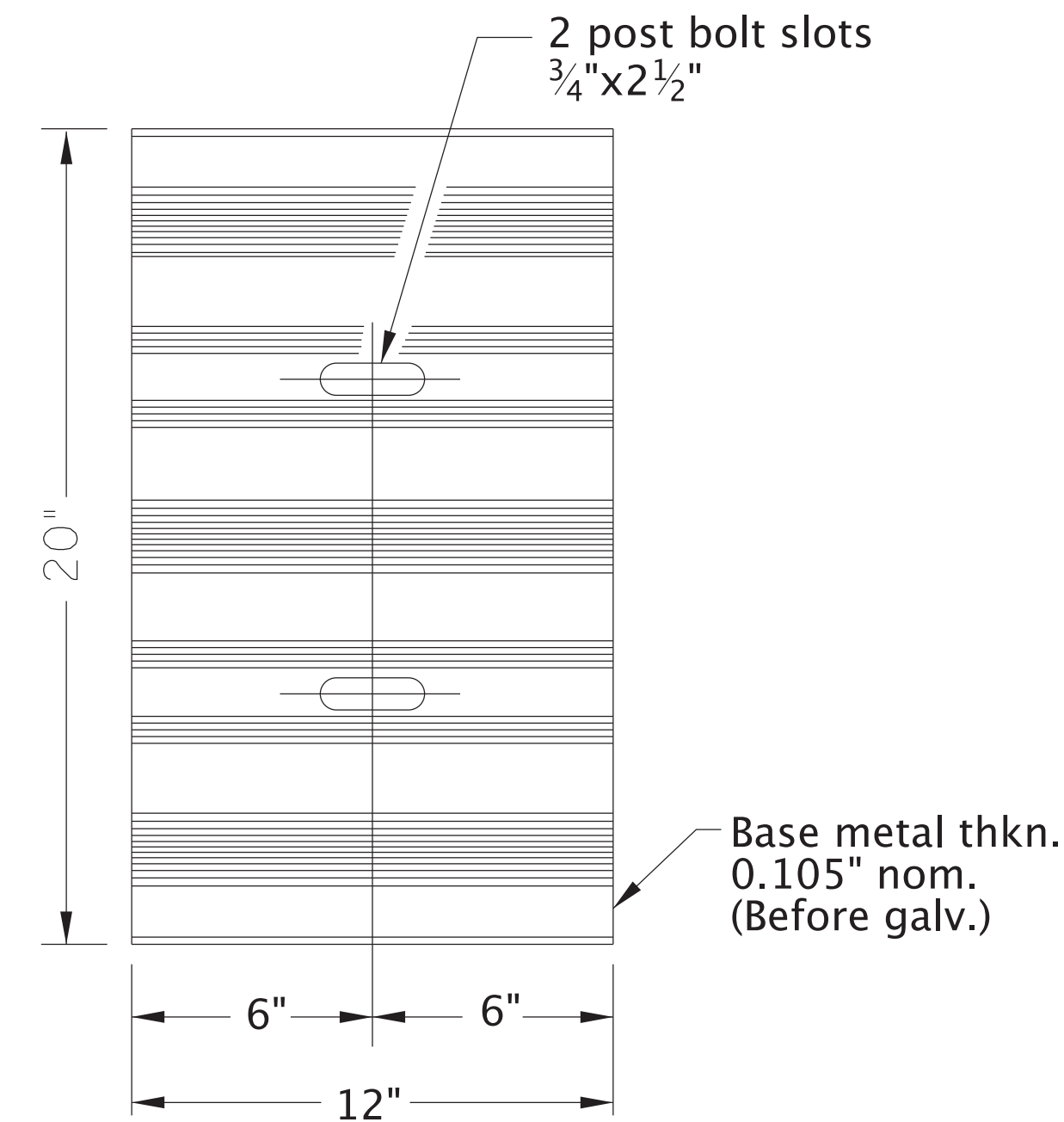
DELINEATOR DETAIL

DELINEATOR NOTES

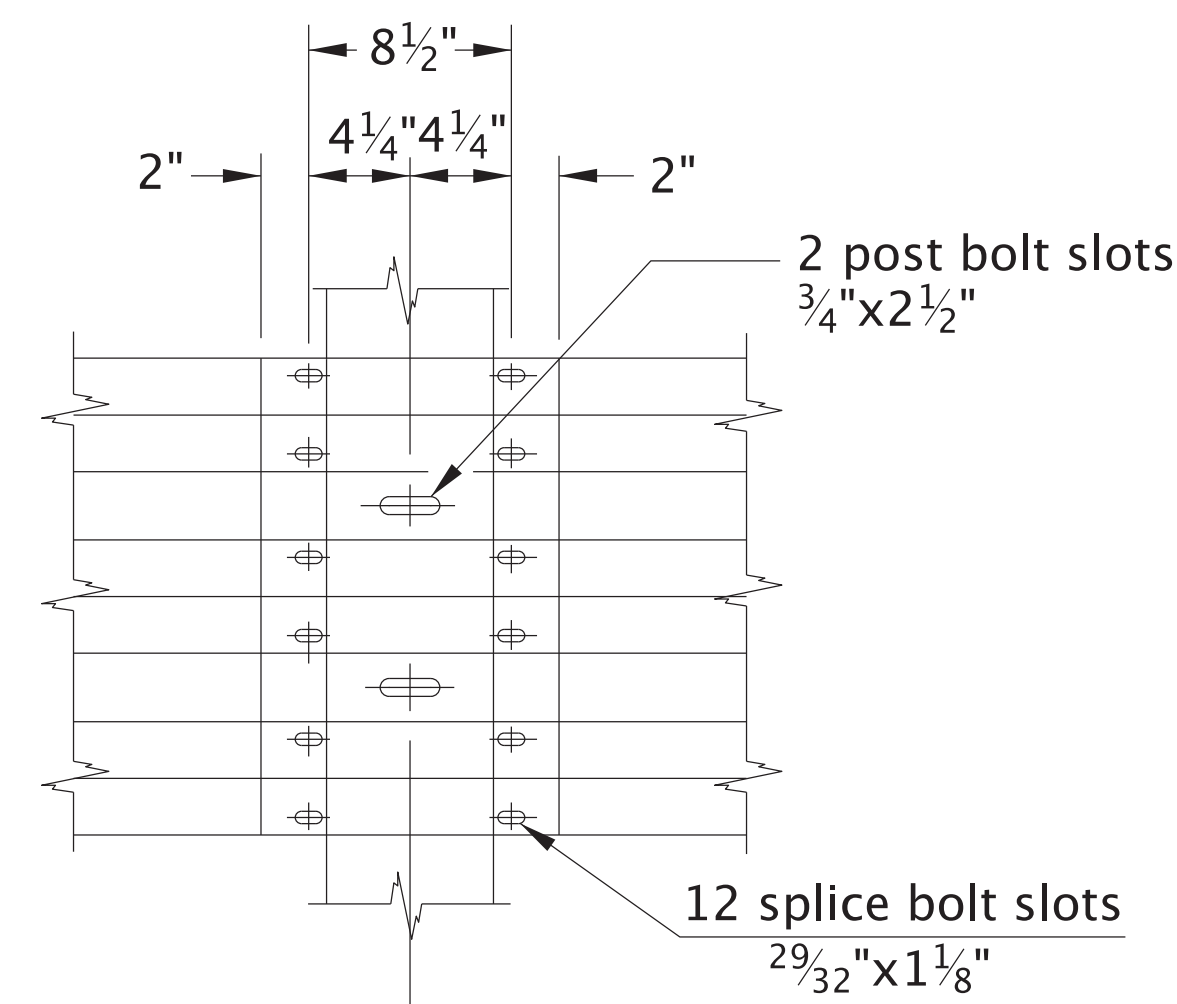
1. Delineators shall be formed of 0.080 poly-carbonate or 0.080 sheet aluminum in accordance with M.18.13.
 2. Retroreflective sheeting shall conform to M.18.09.
 3. Delineators shall be installed on the post closest to the designated spacing.
 4. Retroreflective sheeting shall be white except on the left side of divided streets, highways, ramps, and one way roads in the direction of travel where it shall be yellow.
 5. Install delineators on rail that is parallel to and not greater than 8' from the edge of the roadway. A minimum of three delineators must be installed on any run of rail.
- Delineator Spacing:
 Radius \geq 300' - space every 50'
 Radius $<$ 300' - space every 25'



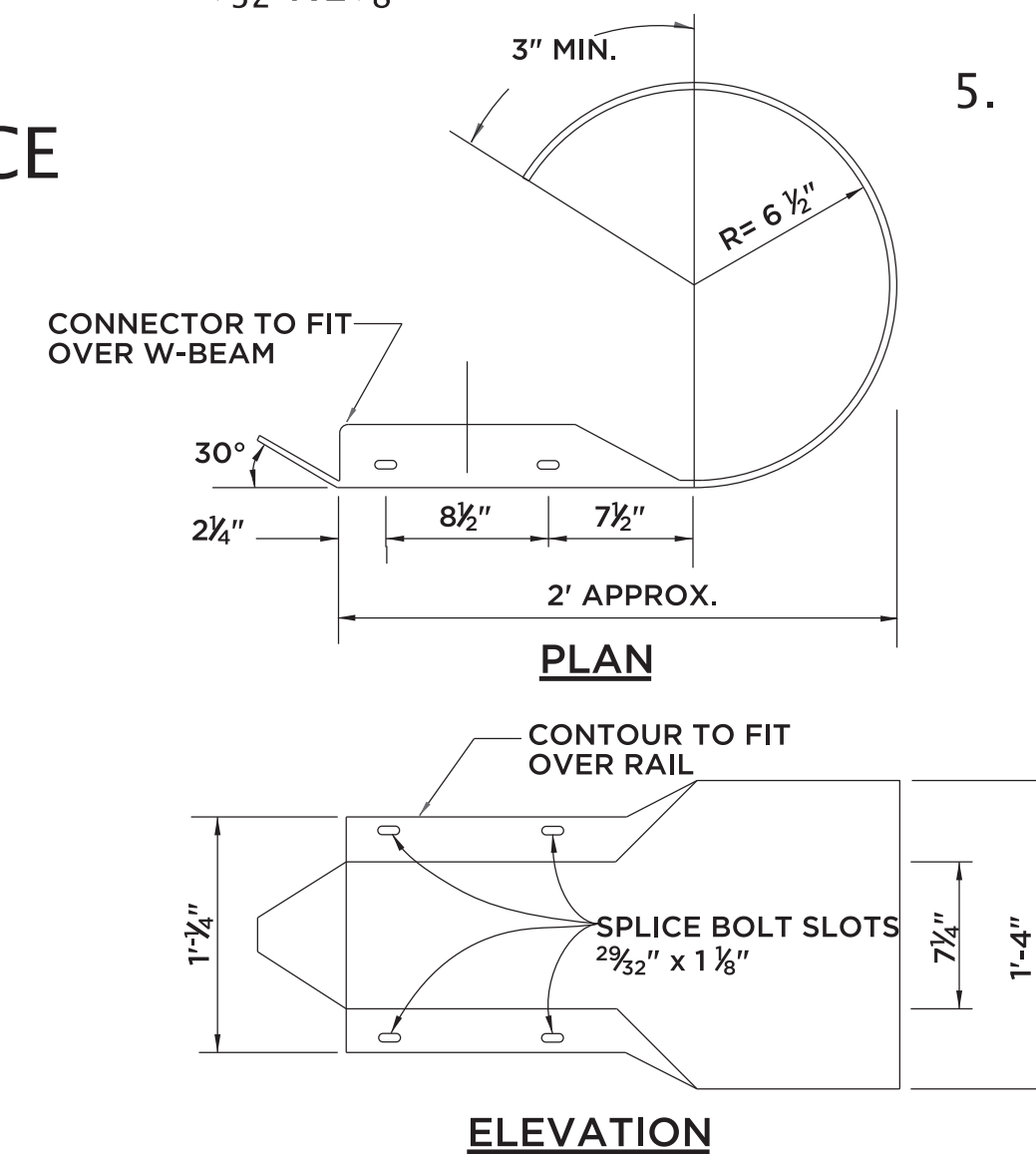
SECTION THRU RAIL ELEMENT



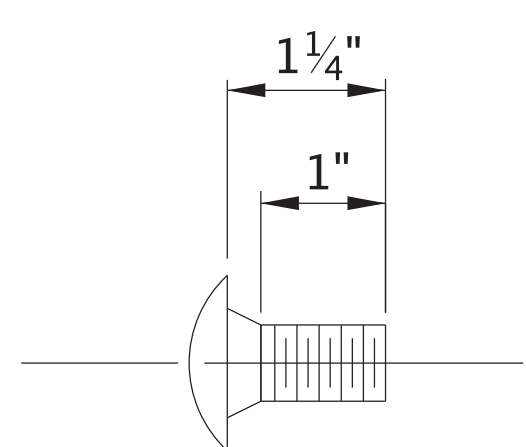
THRIE BEAM BACK-UP PLATE
 For detail not shown, see "Section Thru Rail Element"



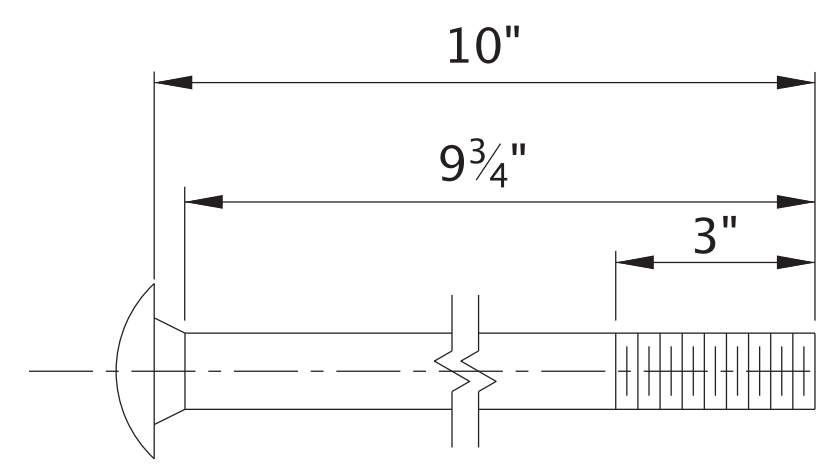
BEAM SPLICE



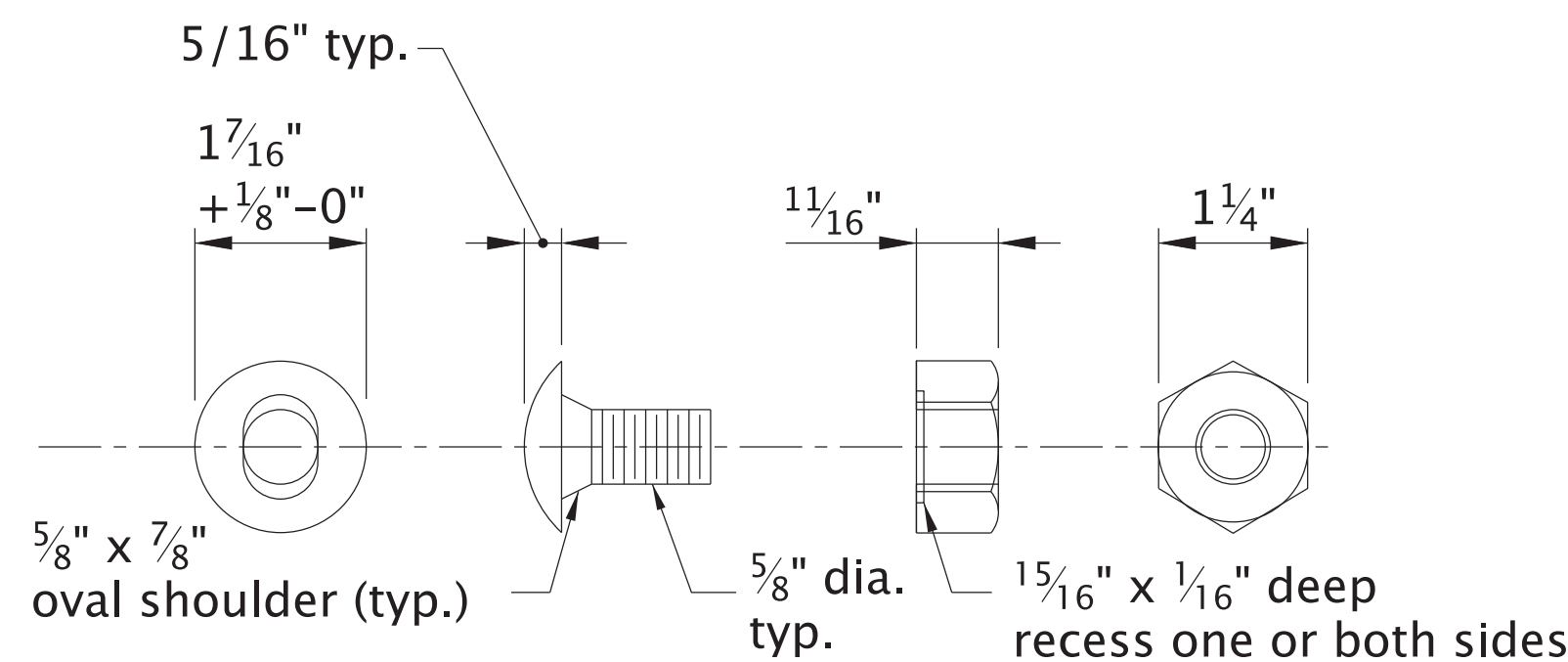
ROUNDED W-BEAM END SECTION



SPLICE BOLT DETAIL



POST BOLT DETAIL
 (unthreaded portion not to exceed 6 3/4")



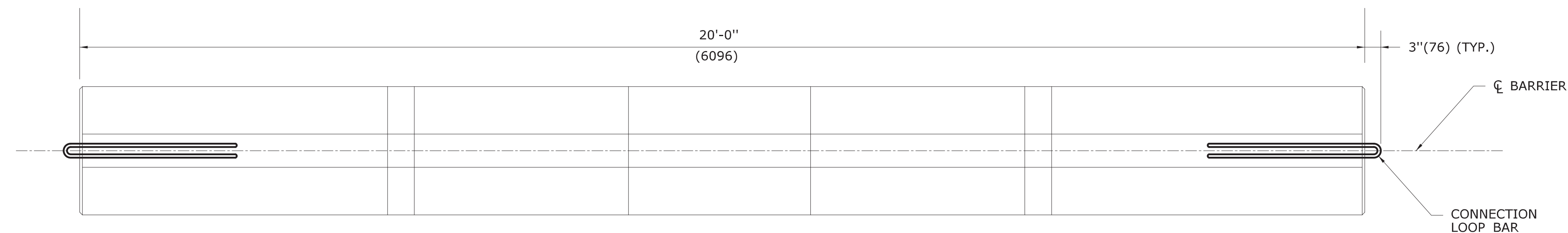
BUTTONHEAD BOLT HEX NUT

Note: After galvanizing, the nut shall be free running on the bolt. Diameter shown is typical for all guiderail bolts. See details for specific lengths.

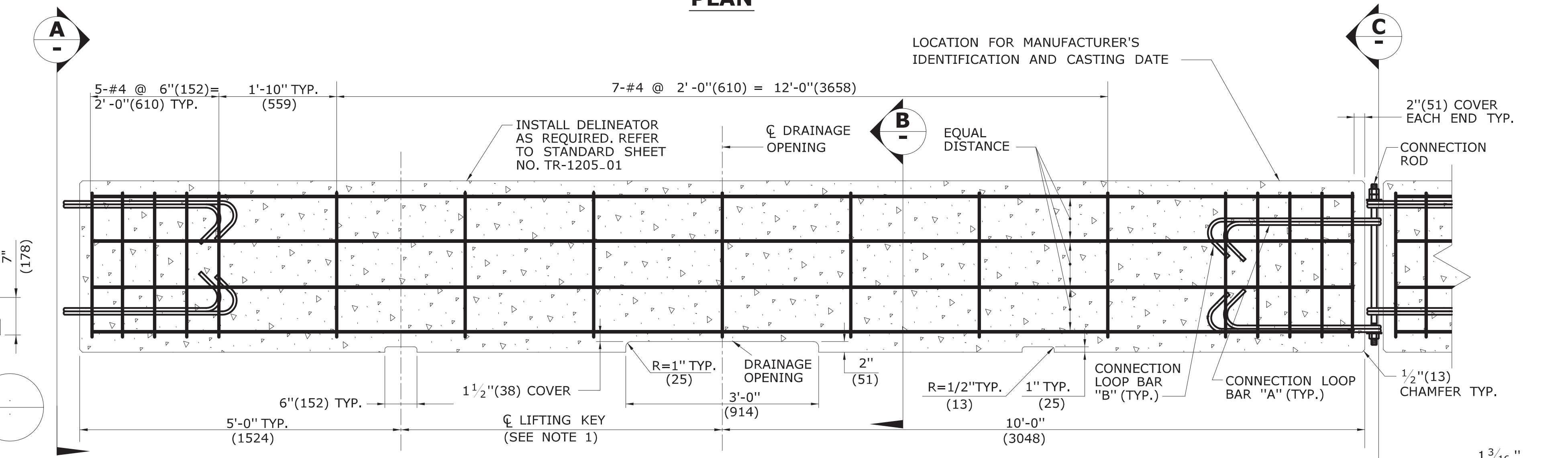
<p>ANCHOR ENGINEERING SERVICES, INC.</p> <p>41 Sequin Drive Glastonbury, CT 06033 Phone: (860) 633-9770 Fax: (860) 633-9971 www.anchorengr.com</p>		<p>REHABILITATION OF BRIDGE 05528</p> <p>PREPARED FOR THE TOWN OF COLCHESTER</p> <p>BRIDGE RAIL & TRANSITION 2 OF 2</p>	
		<p>PAPER MILL ROAD COLCHESTER, CT</p>	
<p>PROJ. ENGINEER KBF</p> <p>PROJ. MANAGER MMZ</p> <p>OFFICE REVIEW SMM</p>	<p>PROJ. DATE</p> <p>084-13 02/11/20</p>	<p>SHEET NO. 8 OF 8</p>	<p>SCALE: NOT TO SCALE</p>

GENERAL NOTES:

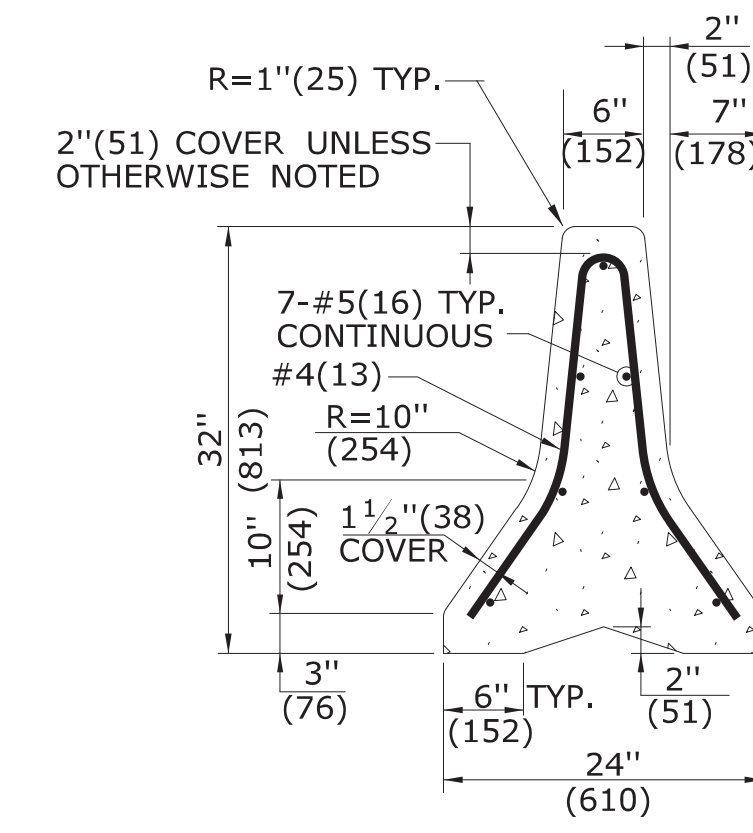
1. ALTERNATE DESIGNS FOR LIFTING KEYS, HOLES OR OTHER HANDLING DEVICES MAY BE SUBMITTED TO THE ENGINEER FOR APPROVAL.
2. EXPECTED PERMANENT DYNAMIC DEFLECTION IS 3'-6" (1148) BASED ON TL-3 CRASH TESTS WITH 240' (73152) OF TPCBC.



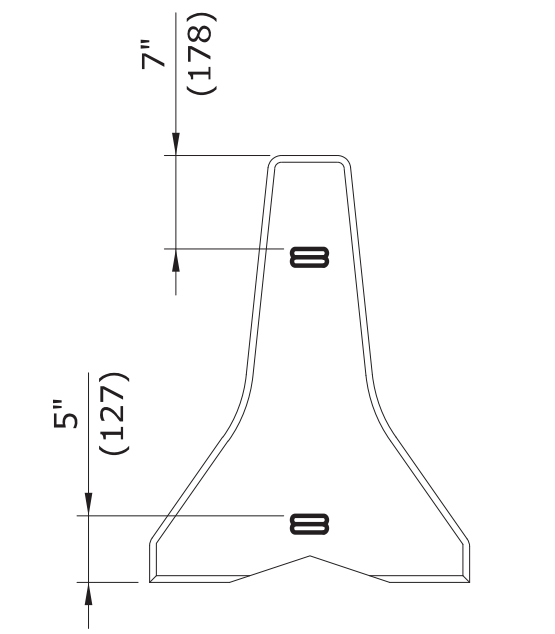
PLAN



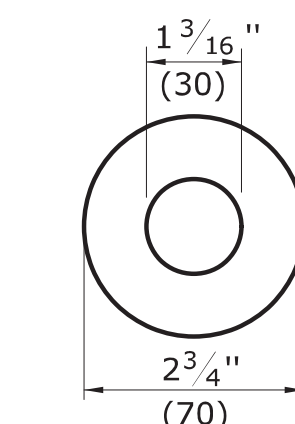
ELEVATION



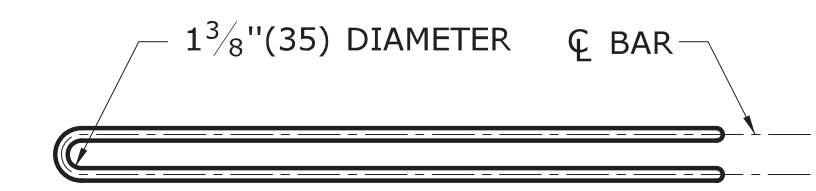
SECTION B



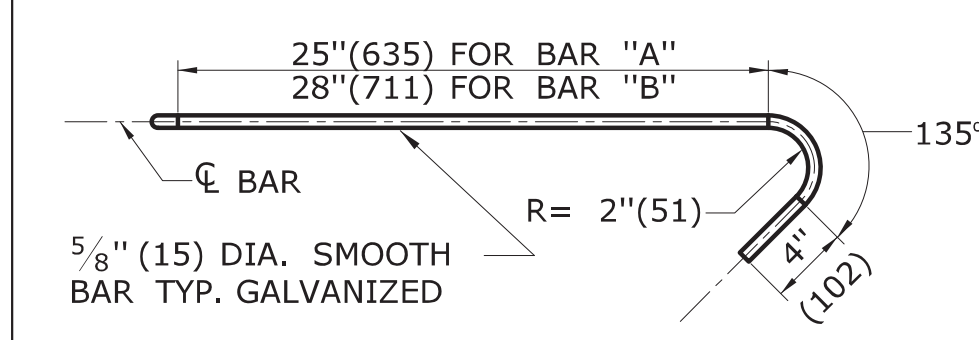
END VIEW C



WASHER DETAIL

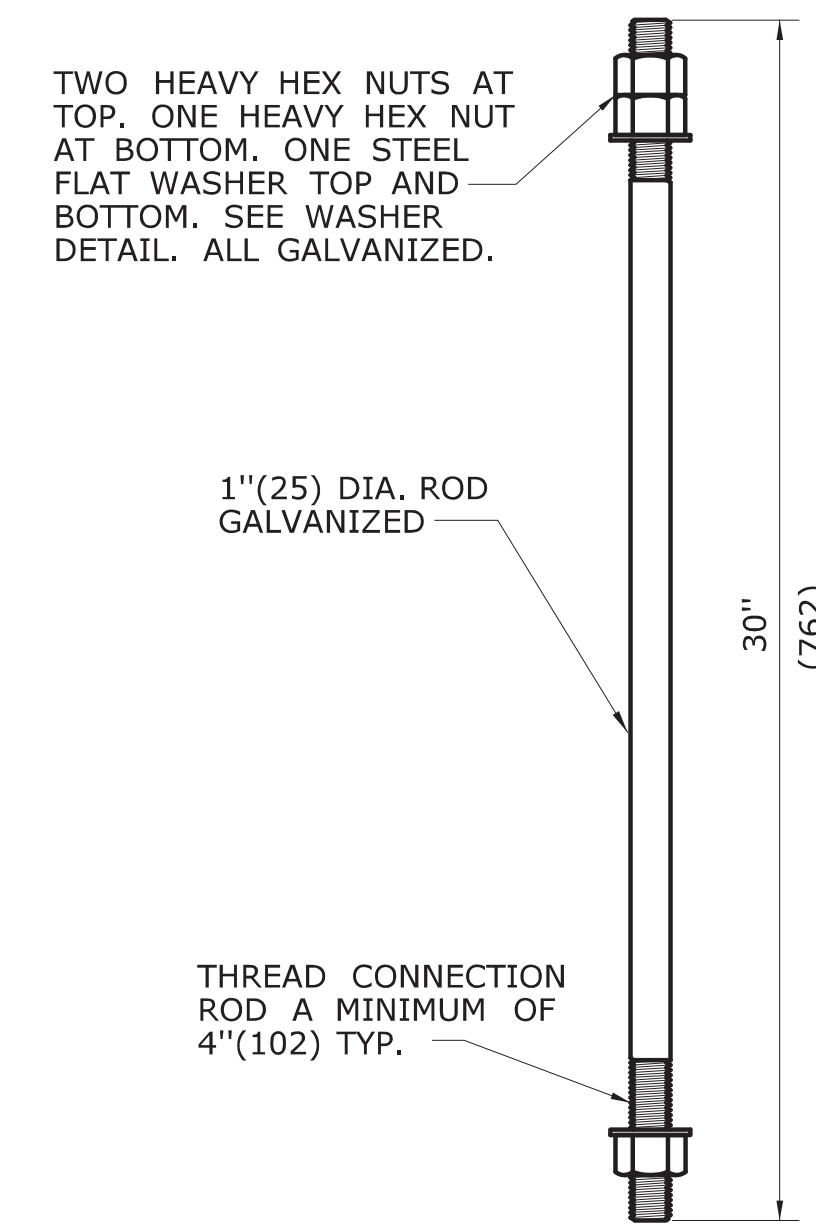


PLAN

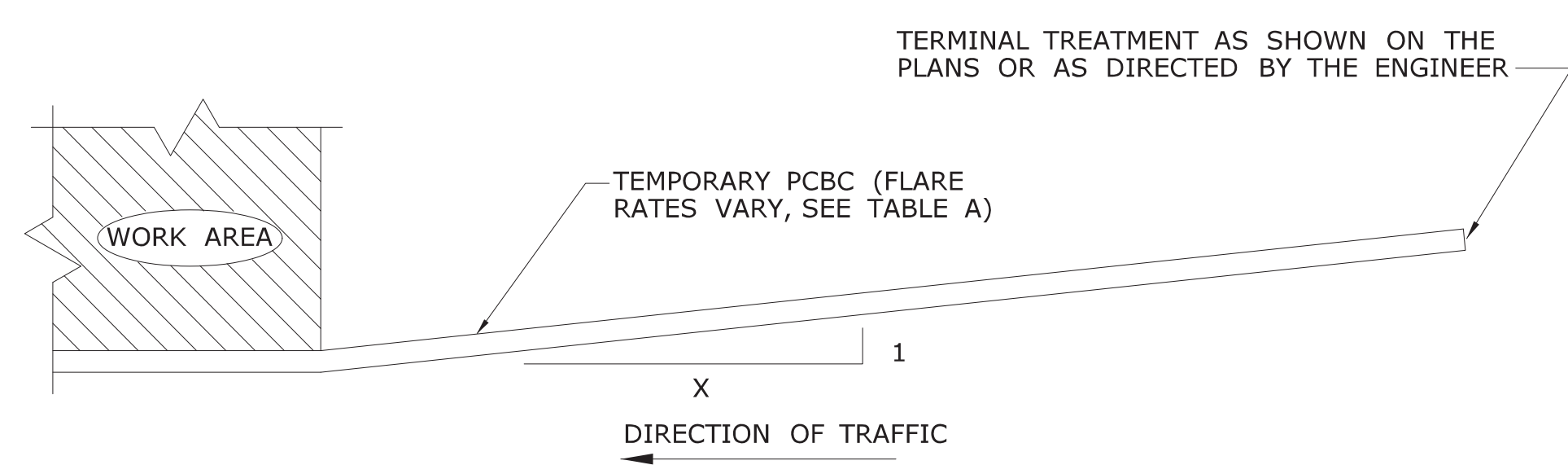


ELEVATION

CONNECTION LOOP BAR



CONNECTION ROD



PLAN - TYPICAL INSTALLATION

TABLE A	
FLARE RATES	
* SPEED	FLARE RATE (X : 1)
≤ 30MPH(48KPH)	4 : 1
> 30MPH(48KPH) < 45MPH(72KPH)	6 : 1
> 45MPH(72KPH) NON-LIMITED ACCESS HIGHWAYS	8 : 1
ALL LIMITED ACCESS HIGHWAYS	10 : 1

* DESIGN SPEED THROUGH THE WORK AREA.

TERMINAL TREATMENT AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER

ALL METRIC DIMENSIONS ARE IN MILLIMETERS (mm) UNLESS OTHERWISE NOTED.

1	3/12	REVISE SLOPE NOTE & DETAIL NOTE					
2	7/13	ERRATA					
-	-	-					
-	-	-					
-	-	-					
-	-	-					
-	-	-					

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

NOT TO SCALE

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

NAME/DATE/TIME:
Leo L. Fontaine
Digitally signed by Leo L. Fontaine
Date: 2013.07.24 11:00:01-04'00'

APPROVED BY:
James H. Norman
NAME/DATE/TIME:
James H. Norman
2013.07.24 14:48:34-04'00'

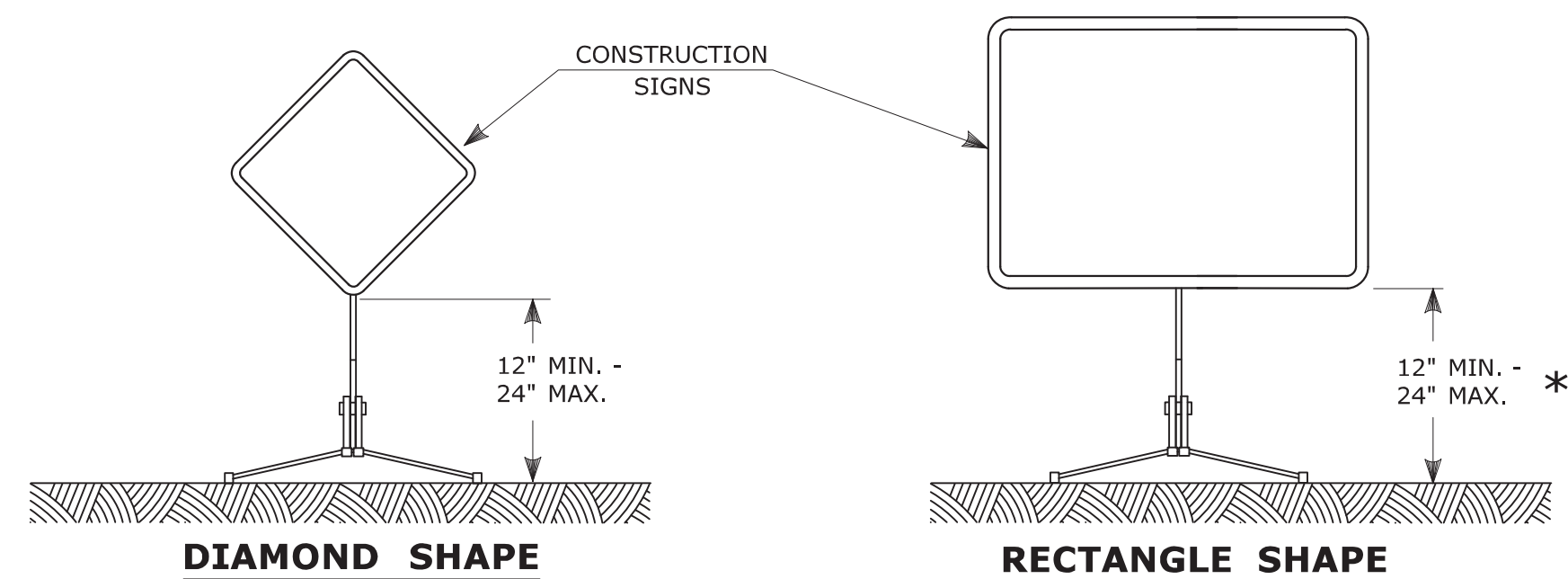
CTDOT
STANDARD SHEET

OFFICE OF ENGINEERING

TEMPORARY PRECAST
CONCRETE BARRIER CURB

HW-822_01

E5 - SERIES				G20 - SERIES				M4 - SERIES				R1 - SERIES				R9 & R11 - SERIES				W1 - SERIES				W3 - SERIES																																																																																																																														
<p>COPY & BORDER - WHITE BACKGROUND - GREEN</p> <table border="1"> <thead> <tr> <th>AREA (SQ. FT)</th> <th>SIZE (INCHES)</th> <th>CONN. D.O.T. #</th> <th>POSTS</th> </tr> </thead> <tbody> <tr> <td>16.0</td> <td>48</td> <td>51-6147</td> <td>2</td> </tr> </tbody> </table>				AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	16.0	48	51-6147	2	<p>END ROAD WORK</p> <table border="1"> <thead> <tr> <th>AREA (SQ. FT)</th> <th>SIZE (INCHES)</th> <th>CONN. D.O.T. #</th> <th>POSTS</th> </tr> </thead> <tbody> <tr> <td>8.0</td> <td>48X24</td> <td>80-9612</td> <td>2</td> </tr> </tbody> </table>				AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	8.0	48X24	80-9612	2	<p>DETOUR</p> <table border="1"> <thead> <tr> <th>AREA (SQ. FT)</th> <th>SIZE (INCHES)</th> <th>CONN. D.O.T. #</th> <th>POSTS</th> </tr> </thead> <tbody> <tr> <td>2.0</td> <td>24X12</td> <td>80-9707</td> <td>1</td> </tr> </tbody> </table>				AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	2.0	24X12	80-9707	1	<p>STOP</p> <table border="1"> <thead> <tr> <th>AREA (SQ. FT)</th> <th>SIZE (INCHES)</th> <th>CONN. D.O.T. #</th> <th>POSTS</th> </tr> </thead> <tbody> <tr> <td>13.30</td> <td>30</td> <td>31-0552</td> <td>2</td> </tr> </tbody> </table>				AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	13.30	30	31-0552	2	<p>SIDEWALK CLOSED</p> <table border="1"> <thead> <tr> <th>AREA (SQ. FT)</th> <th>SIZE (INCHES)</th> <th>CONN. D.O.T. #</th> <th>POSTS</th> </tr> </thead> <tbody> <tr> <td>3.75</td> <td>30X18</td> <td>80-9076</td> <td>1</td> </tr> </tbody> </table>				AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	3.75	30X18	80-9076	1	<p>ROAD CLOSED 00 MILES AHEAD LOCAL TRAFFIC ONLY</p> <table border="1"> <thead> <tr> <th>AREA (SQ. FT)</th> <th>SIZE (INCHES)</th> <th>CONN. D.O.T. #</th> <th>POSTS</th> </tr> </thead> <tbody> <tr> <td>12.5</td> <td>60X30</td> <td>80-9077</td> <td>2</td> </tr> </tbody> </table>				AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	12.5	60X30	80-9077	2	<p>(L) (R)</p> <table border="1"> <thead> <tr> <th>AREA (SQ. FT)</th> <th>SIZE (INCHES)</th> <th>CONN. D.O.T. #</th> <th>POSTS</th> </tr> </thead> <tbody> <tr> <td>9.0</td> <td>36</td> <td>80-9432L</td> <td>1</td> </tr> <tr> <td>9.0</td> <td>36</td> <td>80-9431R</td> <td>1</td> </tr> <tr> <td>16.0</td> <td>48</td> <td>80-9452L</td> <td>2</td> </tr> <tr> <td>16.0</td> <td>48</td> <td>80-9451R</td> <td>2</td> </tr> </tbody> </table>				AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	9.0	36	80-9432L	1	9.0	36	80-9431R	1	16.0	48	80-9452L	2	16.0	48	80-9451R	2	<p>(L) (R)</p> <table border="1"> <thead> <tr> <th>AREA (SQ. FT)</th> <th>SIZE (INCHES)</th> <th>CONN. D.O.T. #</th> <th>POSTS</th> </tr> </thead> <tbody> <tr> <td>16.0</td> <td>48</td> <td>80-9433L</td> <td>2</td> </tr> <tr> <td>16.0</td> <td>48</td> <td>80-9435R</td> <td>2</td> </tr> </tbody> </table>				AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	16.0	48	80-9433L	2	16.0	48	80-9435R	2	<p>(L) (R)</p> <table border="1"> <thead> <tr> <th>AREA (SQ. FT)</th> <th>SIZE (INCHES)</th> <th>CONN. D.O.T. #</th> <th>POSTS</th> </tr> </thead> <tbody> <tr> <td>25.0</td> <td>60</td> <td>80-9483L</td> <td>2</td> </tr> <tr> <td>25.0</td> <td>60</td> <td>80-9485R</td> <td>2</td> </tr> </tbody> </table>				AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	25.0	60	80-9483L	2	25.0	60	80-9485R	2	<p>W3-1</p> <table border="1"> <thead> <tr> <th>AREA (SQ. FT)</th> <th>SIZE (INCHES)</th> <th>CONN. D.O.T. #</th> <th>POSTS</th> </tr> </thead> <tbody> <tr> <td>9.0</td> <td>36</td> <td>80-9050</td> <td>1</td> </tr> <tr> <td>16.0</td> <td>48</td> <td>80-9051</td> <td>2</td> </tr> </tbody> </table>				AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	9.0	36	80-9050	1	16.0	48	80-9051	2							
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(R)	16.0	48	80-9848	2																																																																																																																																																		
AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS																																																																																																																																																			
2.0	24X12	80-9870																																																																																																																																																				
3.75	30X18	80-9871																																																																																																																																																				
AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS																																																																																																																																																			
10.5	42X36	80-9621	2																																																																																																																																																			
AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS																																																																																																																																																			
(1)	16.0	48	80-9957	2																																																																																																																																																		
(2)	9.0	36	80-9958	1																																																																																																																																																		
(2)	16.0	48	80-9959	2																																																																																																																																																		
<p>NEW</p> <table border="1"> <thead> <tr> <th>AREA (SQ. FT)</th> <th>SIZE (INCHES)</th> <th>CONN. D.O.T. #</th> <th>POSTS</th> </tr> </thead> <tbody> <tr> <td>2.0</td> <td>24X12</td> <td>80-9049</td> <td></td> </tr> </tbody> </table>				AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	2.0	24X12	80-9049		<p>ONE LANE ROAD AHEAD</p> <table border="1"> <thead> <tr> <th>AREA (SQ. FT)</th> <th>SIZE (INCHES)</th> <th>CONN. D.O.T. #</th> <th>POSTS</th> </tr> </thead> <tbody> <tr> <td>9.0</td> <td>36</td> <td>80-9834</td> <td>1</td> </tr> <tr> <td>16.0</td> <td>48</td> <td>80-9835</td> <td>2</td> </tr> </tbody> </table>				AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	9.0	36	80-9834	1	16.0	48	80-9835	2	<p>LEFT LANE CLOSED</p> <table border="1"> <thead> <tr> <th>AREA (SQ. FT)</th> <th>SIZE (INCHES)</th> <th>CONN. D.O.T. #</th> <th>POSTS</th> </tr> </thead> <tbody> <tr> <td>(L)</td> <td>16.0</td> <td>48</td> <td>80-9846</td> <td>2</td> </tr> <tr> <td>(R)</td> <td>16.0</td> <td>48</td> <td>80-9849</td> <td>2</td> </tr> </tbody> </table>				AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	(L)	16.0	48	80-9846	2	(R)	16.0	48	80-9849	2	<p>RIGHT LANE CLOSED</p> <table border="1"> <thead> <tr> <th>AREA (SQ. FT)</th> <th>SIZE (INCHES)</th> <th>CONN. D.O.T. #</th> <th>POSTS</th> </tr> </thead> <tbody> <tr> <td>(L)</td> <td>16.0</td> <td>48</td> <td>80-9846</td> <td>2</td> </tr> <tr> <td>(R)</td> <td>16.0</td> <td>48</td> <td>80-9849</td> <td>2</td> </tr> </tbody> </table>				AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	(L)	16.0	48	80-9846	2	(R)	16.0	48	80-9849	2	<p>1 MILE</p> <table border="1"> <thead> <tr> <th>AREA (SQ. FT)</th> <th>SIZE (INCHES)</th> <th>CONN. D.O.T. #</th> <th>POSTS</th> </tr> </thead> <tbody> <tr> <td>2.5</td> <td>30X12</td> <td>80-9872</td> <td></td> </tr> <tr> <td>4.5</td> <td>36X18</td> <td>80-9873</td> <td></td> </tr> </tbody> </table>				AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	2.5	30X12	80-9872		4.5	36X18	80-9873		<p>STOP-SLOW PADDLE</p> <table border="1"> <thead> <tr> <th>AREA (SQ. FT)</th> <th>SIZE (INCHES)</th> <th>CONN. D.O.T. #</th> <th>POSTS</th> </tr> </thead> <tbody> <tr> <td>2.25</td> <td>18</td> <td>80-9950</td> <td>PADDLE</td> </tr> </tbody> </table>				AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	2.25	18	80-9950	PADDLE																																																											
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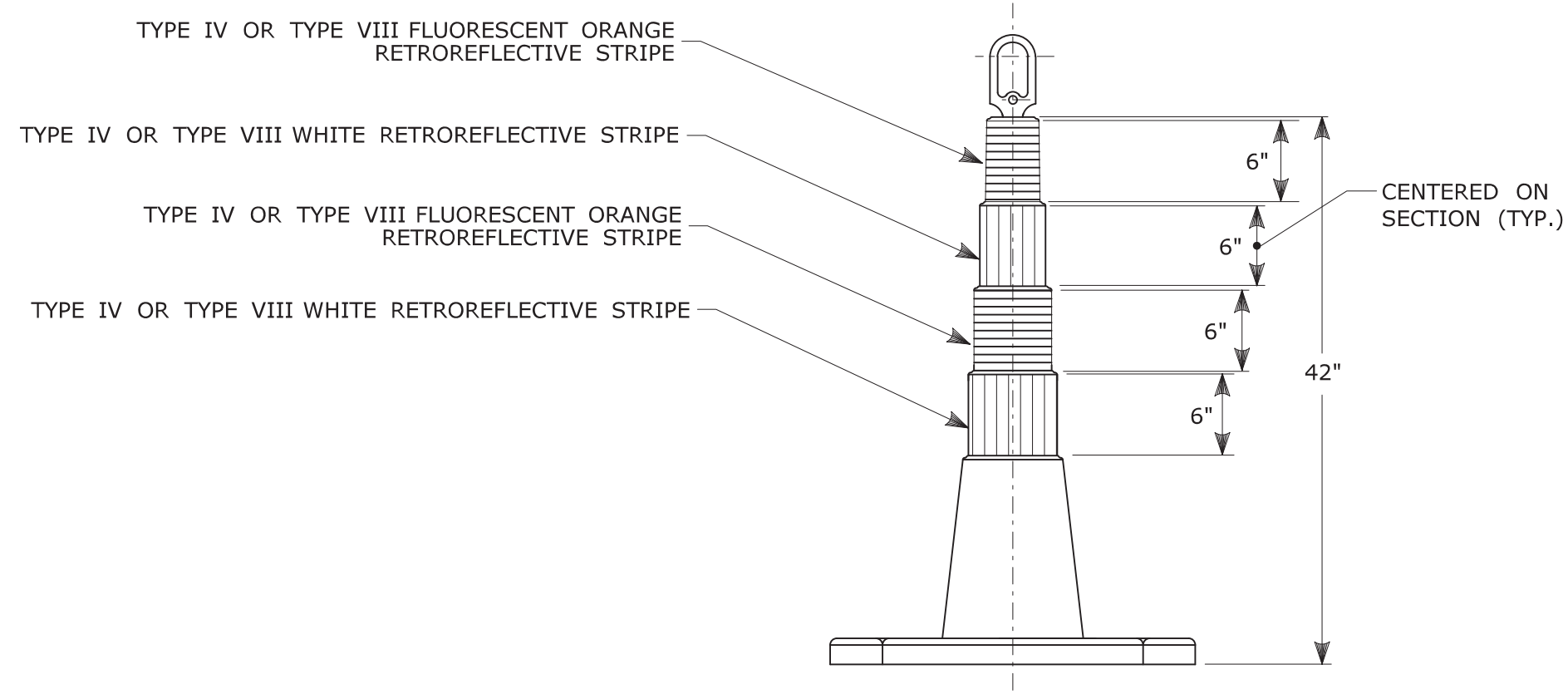


PORTABLE CONSTRUCTION SIGNS

NOTES FOR PORTABLE SIGN SUPPORTS:

- SIGNS AND THEIR PORTABLE SUPPORTS SHALL CONFORM TO THE REQUIREMENTS OF NCHRP REPORT 350 (TL-3) OR THE AASHTO MASH FOR CATEGORY 2 DEVICES AND THE LATEST EDITION OF THE MUTCD.
- MOUNTING HEIGHT OF SIGNS SHALL BE A MINIMUM OF 12" AND A MAXIMUM OF 24". SIGNS SHALL BE MOUNTED HIGHER AS NEEDED TO MEET FIELD CONDITIONS OR AS DIRECTED BY THE ENGINEER.
- THE ENGINEER RESERVES THE RIGHT TO REJECT ANY SUPPORT DEEMED UNSUITABLE FOR THE PURPOSE INTENDED.
- PORTABLE SIGN SUPPORTS SHALL BE STABILIZED IN A MANNER THAT WILL NOT AFFECT THEIR COMPLIANCE WITH NCHRP REPORT 350 (TL-3) OR THE AASHTO MASH FOR CATEGORY 2 DEVICES.
- PORTABLE CONSTRUCTION SIGN SUPPORTS SHOULD NOT BE USED FOR DURATION OF MORE THAN 3 DAYS EXCEPT FOR R9-8 THROUGH R9-11a SERIES, R11 SERIES, W1-6 THROUGH W1-8 SERIES, M4-10, AND E5-1. SEE STANDARD SHEET TR-1220.01 - "SIGNS FOR CONSTRUCTION AND PERMIT OPERATIONS" FOR SIGN DETAILS.

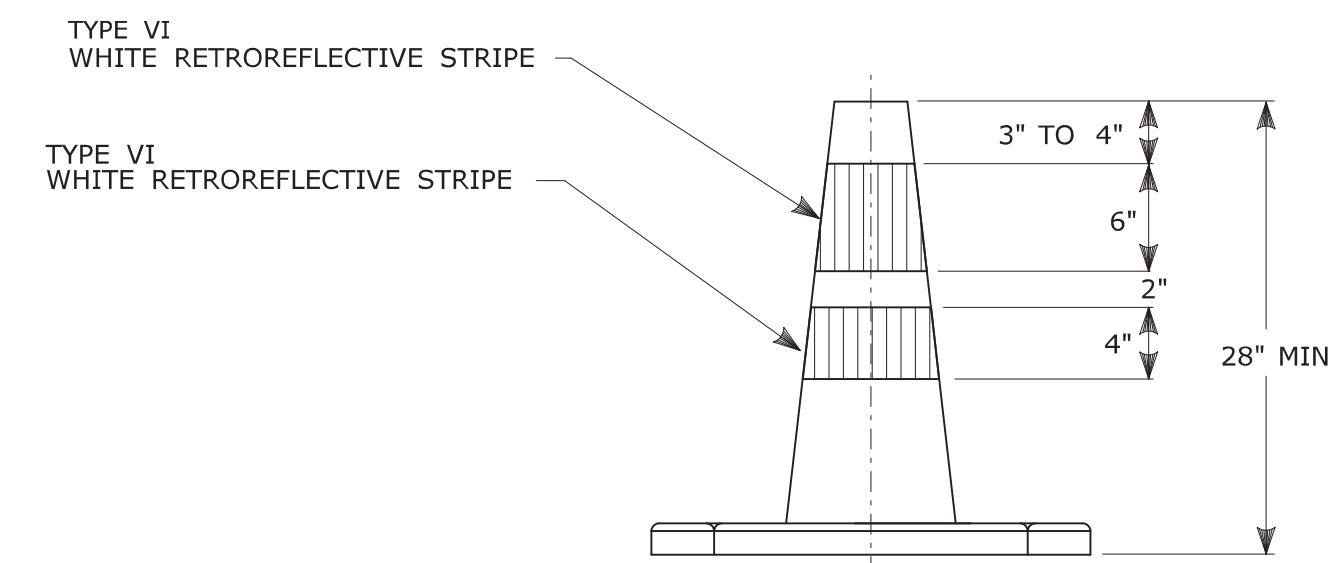
* FOR E5-1 (EXIT SIGNS) USE MIN 48".



42" TRAFFIC CONE

NOTES:

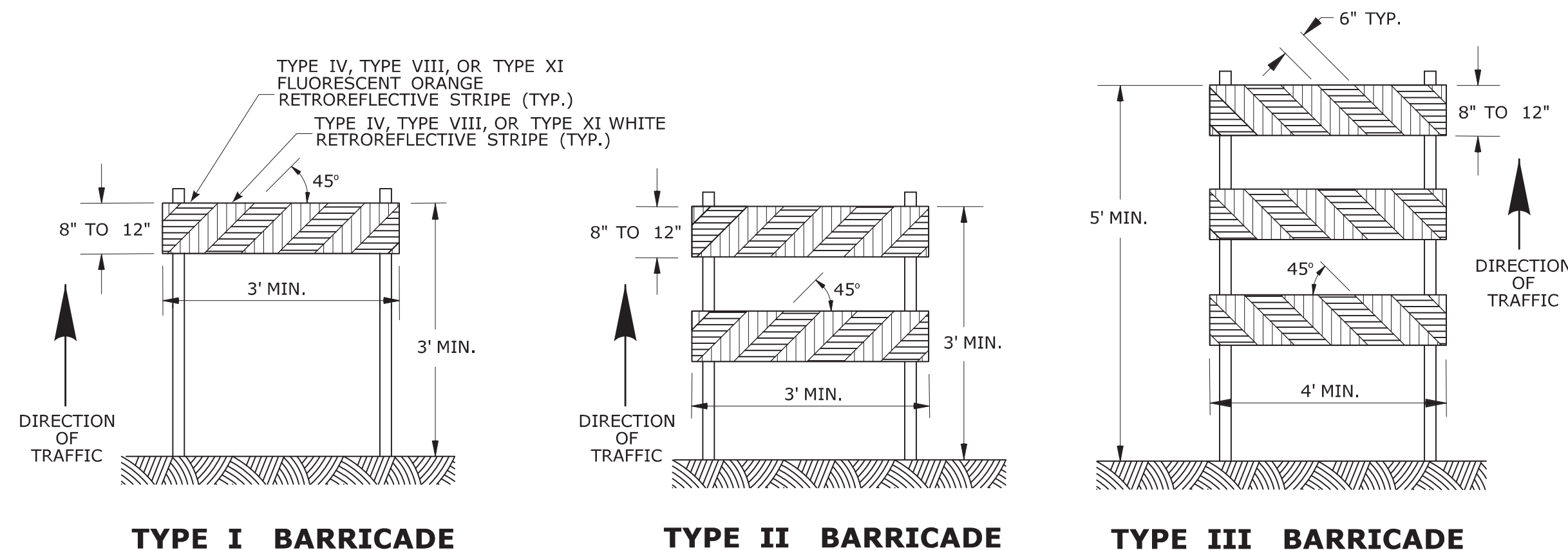
- TRAFFIC CONES SHALL CONFORM TO THE REQUIREMENTS OF NCHRP REPORT 350 (TL-3) OR THE AASHTO MASH FOR CATEGORY 1 DEVICES AND THE LATEST EDITION OF THE MUTCD.
- IF RUBBER CONES ARE USED, THEY SHALL HAVE INTERIOR RIBS FOR RIGIDITY.
- IF PLASTIC CONES ARE USED, THEY SHALL BE COLOR IMPREGNATED.
- THE ENGINEER RESERVES THE RIGHT TO REJECT ANY CONE DEEMED UNSUITABLE FOR THE PURPOSE INTENDED.
- THE ENTIRE AREA OF FLUORESCENT ORANGE AND WHITE STRIPES SHALL BE RETROREFLECTIVE SHEETING AS REQUIRED IN THE SPECIFICATIONS.
- THE SECTIONS OF CONES NOT COVERED WITH RETROREFLECTIVE STRIPES SHALL BE ORANGE.



TRAFFIC CONE

NOTES:

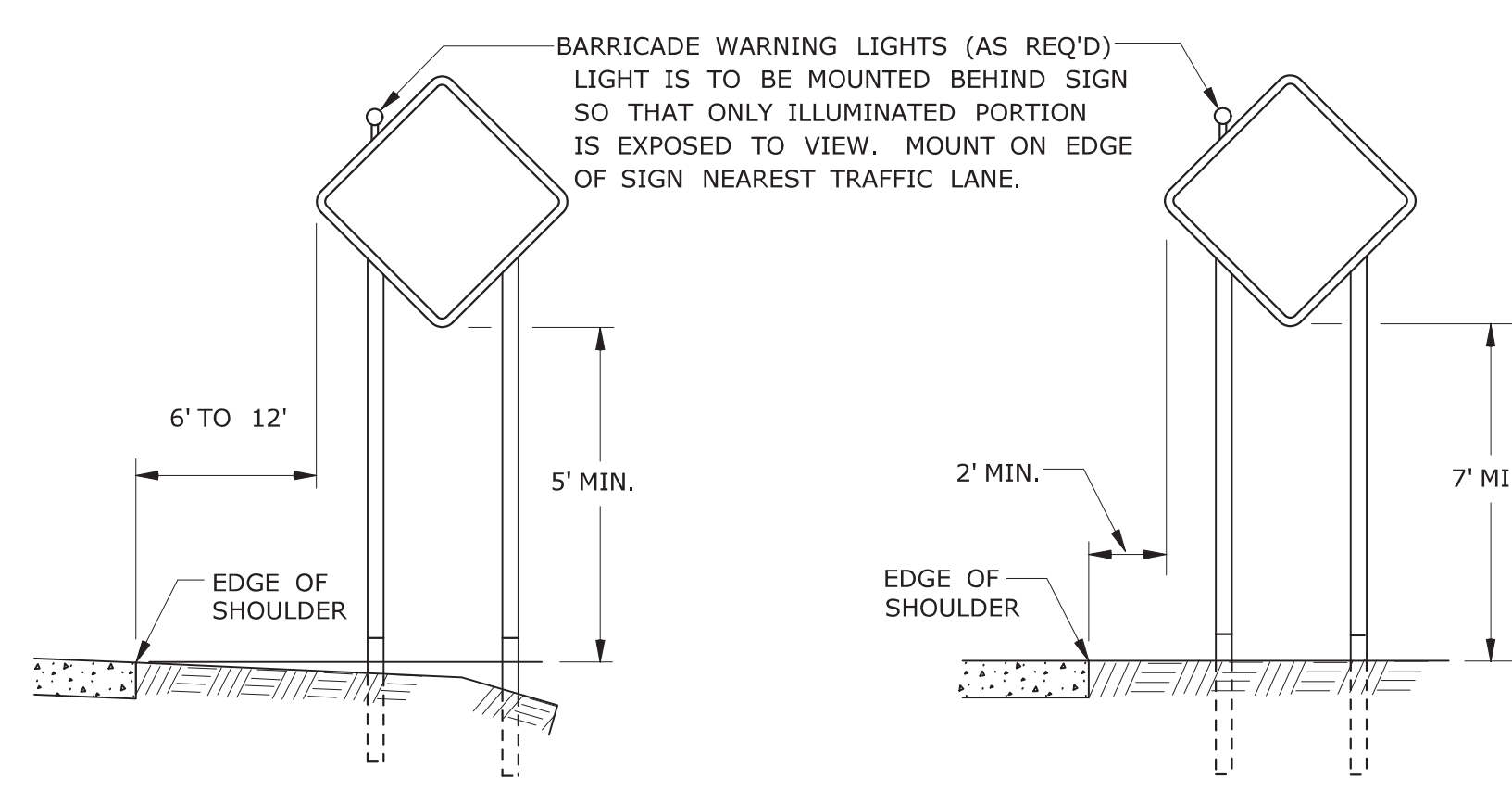
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- IF PLASTIC CONES ARE USED, THEY SHALL BE COLOR IMPREGNATED.
- THE ENGINEER RESERVES THE RIGHT TO REJECT ANY CONE DEEMED UNSUITABLE FOR THE PURPOSE INTENDED.
- THE ENTIRE AREA OF WHITE STRIPES SHALL BE RETROREFLECTIVE SHEETING AS REQUIRED IN THE SPECIFICATIONS.
- TRAFFIC CONES NOT USED AT NIGHT MAY UTILIZE TYPE III SHEETING.
- THE SECTIONS OF CONES NOT COVERED WITH RETROREFLECTIVE STRIPES SHALL BE ORANGE.



CONSTRUCTION BARRICADES

NOTES:

- CONSTRUCTION BARRICADES SHALL CONFORM TO THE REQUIREMENTS OF NCHRP REPORT 350 (TL-3) OR THE AASHTO MASH AND THE LATEST EDITION OF THE MUTCD.
- MARKINGS FOR BARRICADE RAILS SHALL BE ALTERNATE FLUORESCENT ORANGE AND WHITE STRIPES SLOPING DOWNWARD IN THE DIRECTION TRAFFIC IS TO PASS. 6" WIDE STRIPES SHALL BE USED.
- THE ENTIRE AREA OF FLUORESCENT ORANGE AND WHITE STRIPES SHALL BE RETROREFLECTIVE SHEETING AS REQUIRED IN THE SPECIFICATIONS. THE SIDES OF BARRICADES FACING TRAFFIC SHALL HAVE RETROREFLECTIVE RAIL FACES.
- THE ENGINEER RESERVES THE RIGHT TO REJECT ANY BARRICADE DEEMED UNSUITABLE FOR THE PURPOSE INTENDED.
- CORNERS OF BARRICADE RAILS SHALL BE ROUNDED.
- SIGNS MAY ONLY BE INSTALLED ON TYPE III BARRICADES AND SHALL BE PLACED SO AS TO COVER NO MORE THAN ONE BARRICADE RAIL.



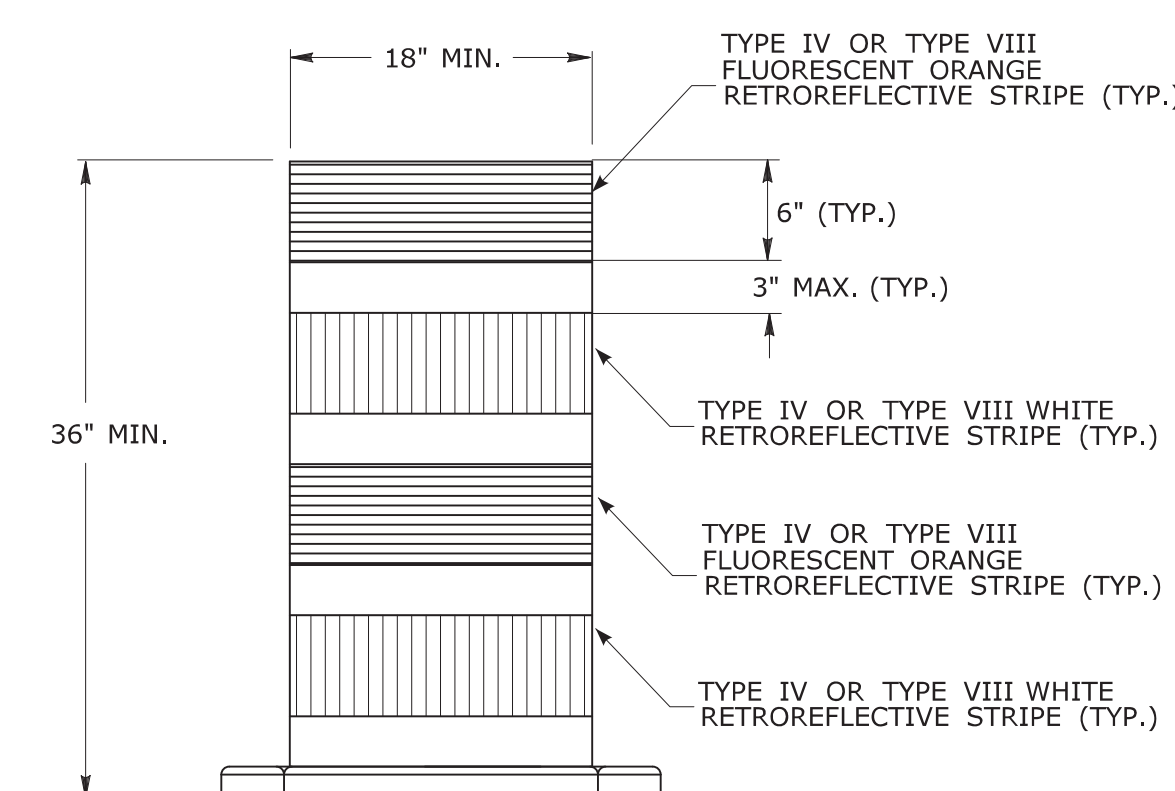
RURAL AREA

URBAN AREA

**PLACEMENT OF CONSTRUCTION SIGNS
TYPICAL LONG TERM INSTALLATION**

NOTES:

- SUPPORTS SHALL BE METAL SIGN POSTS AND HAVE BREAK-AWAY FEATURES.
REFER TO STANDARD SHEETS:
TR-1208.01 - "SIGN PLACEMENT AND RETROREFLECTIVE STRIP DETAILS."
TR-1208.02 - "METAL SIGN POSTS AND SIGN MOUNTING DETAILS."



**TRAFFIC DRUM
FRONT VIEW**

NOTES:

- TRAFFIC DRUM SHALL CONFORM TO THE REQUIREMENTS OF NCHRP REPORT 350 (TL-3) OR THE AASHTO MASH FOR CATEGORY 1 DEVICES AND THE LATEST EDITION OF THE MUTCD.
- THE ENGINEER RESERVES THE RIGHT TO REJECT ANY DRUM DEEMED UNSUITABLE FOR THE PURPOSE INTENDED.
- THE ENTIRE AREA OF FLUORESCENT ORANGE AND WHITE STRIPES SHALL BE RETROREFLECTIVE SHEETING AS REQUIRED IN THE SPECIFICATIONS.
- THE SECTIONS OF DRUMS NOT COVERED WITH RETROREFLECTIVE STRIPES SHALL BE ORANGE.

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED. Plotted Date: 8/10/2018		<p>STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION</p>		SUBMITTED BY: <i>Mark Makuch</i> NAME/DATE/TIME: Mark F. Makuch, P.E. 2018.08.17 09:12:43-04'00' APPROVED BY: <i>YFR</i> NAME/DATE/TIME: Mark F. Carfino, P.E. 2018.08.21 07:49:51-04'00'	CTDOT STANDARD SHEET OFFICE OF ENGINEERING	STANDARD SHEET TITLE: CONSTRUCTION SIGN SUPPORTS AND CHANNELIZING DEVICES	STANDARD SHEET NO.: TR-1220_02
3 8-2018 UPDATED SHEETING TYPE AND COLOR. 2 8-2015 UPDATED PER MUTCD AND FORM 816 JAN 2015 REVISION. 1 2-2011 MINOR REVISIONS.	REV. DATE REVISION DESCRIPTION	NOT TO SCALE	Filename: TR-1220.02.3.2018.dgn Model: TR-1220.02				