

PUB. ROAD DIV. NO.	STATE	TOWN	FED. AID PROJ. NO.	PROJ. NO.	YEAR	ROUTE NO.	SHEET NO.	TOTAL SHEETS
1	CONN.	NEWTOWN & SOUTHBURY	FI 41(8)	96-49	1950	U.S. 6	1	25

CONNECTICUT STATE HIGHWAY DEPARTMENT

PLAN FOR CONSTRUCTION

OF HOUSATONIC RIVER BRIDGE AND APPROACHES ON THE RELOCATION OF ROUTE U.S. 6 IN THE TOWNS OF **NEWTOWN & SOUTHBURY**

FROM STA. 178+23.00 TO STA. 196+40.00
LENGTH 1817.00 FT.

SCALES (PLAN 1 IN. = 40 FT.
PROFILE HOR. 1 IN. = 40 FT. VERT. 1 IN. = 4 FT.
CROSS SECTIONS 1 IN. = 10 FT.

FEDERAL AID PROJECT NO. FI 41(8)

TO BE MAINTAINED BY THE STATE

STAGE ON CONSTRUCTION APPROACH

1947 SPECIFICATIONS FORM NO. 807 GOVERN

TOWN NO. 96
PROJECT NO. 49

ALL ELEVATIONS ON THIS PROJECT BASED ON U.S. SURVEY DATUM

FEDERAL AID PRIMARY SYSTEM
ASSUMED DESIGN SPEED 45 M.P.H.
ADT. 1970 ~ 5000

LIST OF DRAWINGS	
SHEET NO.	TITLE
1	Title Sheet & Estimated Quantities
2	Index Plan & Profile
3	Typical Cross Section Sheet
4-8	Plans & Profiles
9-17	Bridge Sheets
18-24	Cross Section Sheets - Main Line
25	Cross Section Sheet - Fish Rock Road
STANDARD SHEETS	
217B	Wire Rope Railing (Type C) & Compensating Type Anchorages

Construction Started March 5, 1951
Construction Completed December 16, 1953
Plans Revised by G. Ranger Feb 19, 1958
Plans Checked by W. Tolletson Feb. 19, 1958

Approaches Not Submitted with Final Blue Prints

Property Check Completed Feb 19, 1958

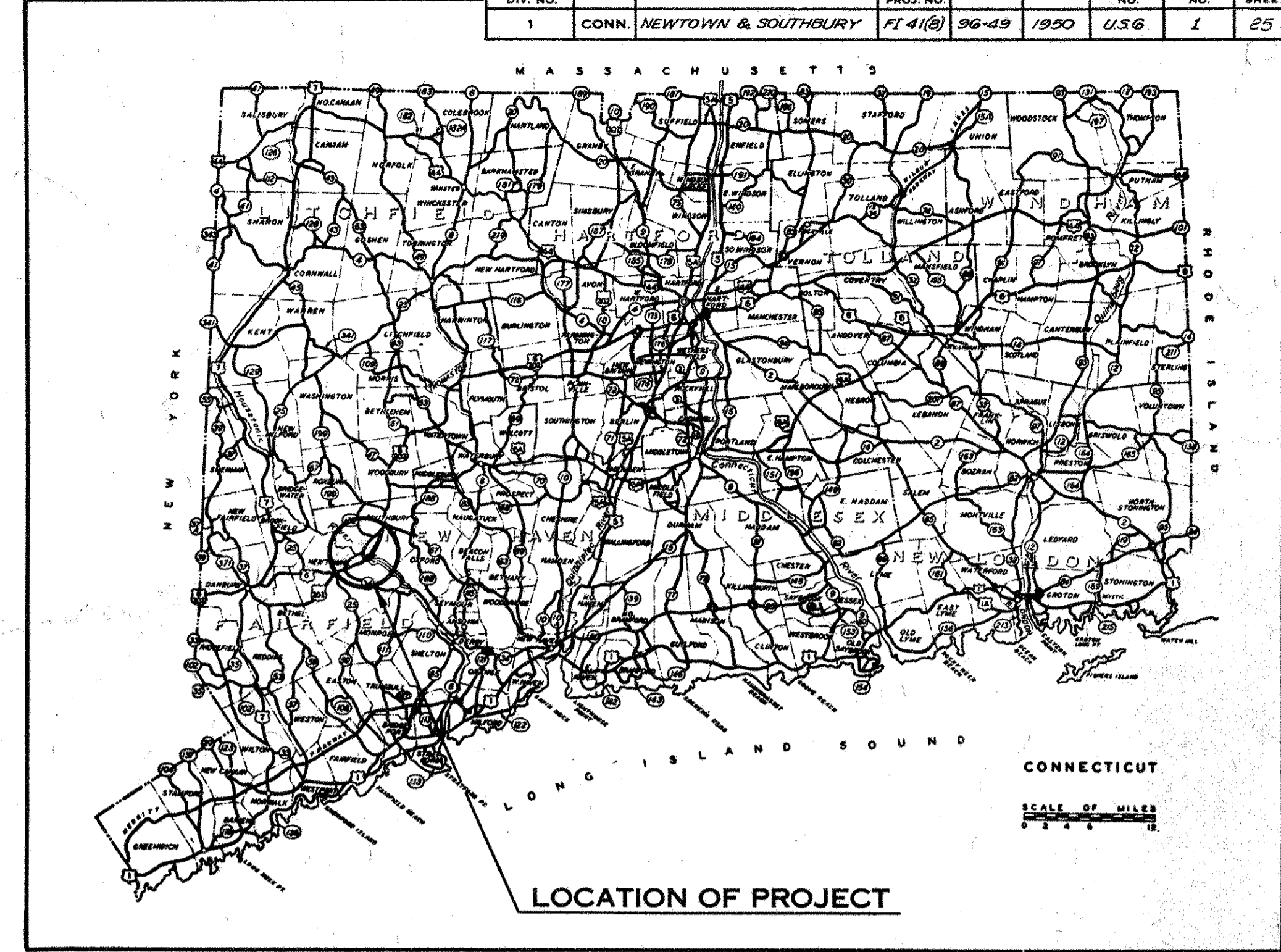
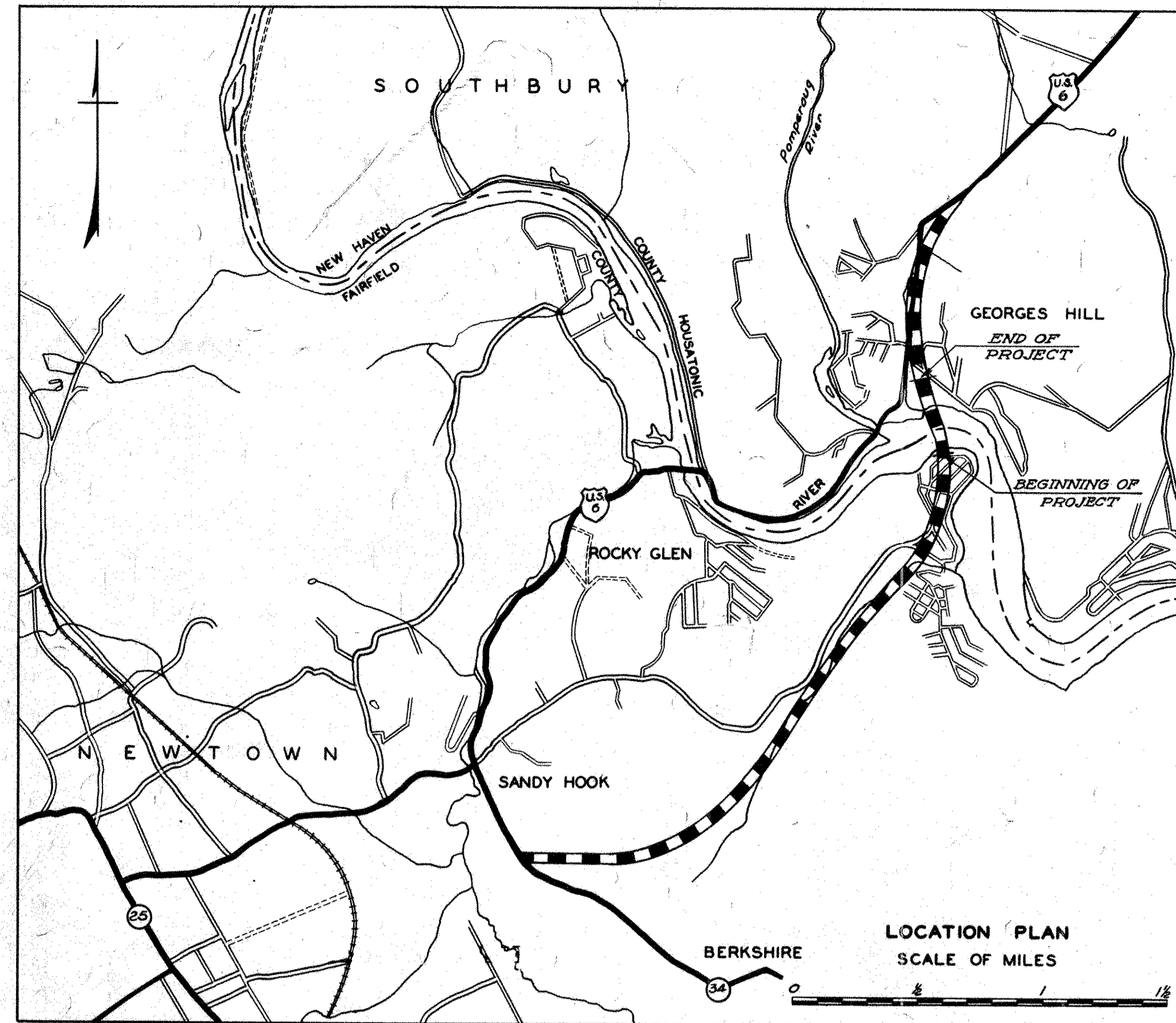
DEPARTMENT OF COMMERCE BUREAU OF PUBLIC ROADS	
RECOMMENDED FOR APPROVAL:	
[Signature]	DATE
DISTRICT ENGINEER	
APPROVED:	
[Signature]	DATE
DIVISION ENGINEER	

APPROVED Oct. 2 1950
Ernest J. Perkins
ENGINEER OF HIGHWAY DESIGN

APPROVED Oct. 3 1950
M. Alfred Kachle
ENGINEER OF LOCATION DESIGN

APPROVED Oct. 3 1950
Roy E. Jorgensen
DEPUTY HIGHWAY COMMISSIONER

ESTIMATED QUANTITIES		
ITEM	UNIT	QUANTITY
Earth Excavation	C.Y.	56579
Rock Excavation	C.Y.	10737
Bridge Excavation ~ No. & So. Abutments	C.Y.	1390
Bridge Excavation ~ Piers 1, 2 & 3	C.Y.	4040
Pile Loading Test	Ea.	2
Furnishing Steel Piles	Lb.	1,921,000
Driving Steel Piles	L.F.	24940
Pile Caps	Ea.	112
Portland Cement	Bbl.	10835
Natural Cement	Bbl.	1706
Class "A" Concrete	C.Y.	6681
Tremie Concrete	C.Y.	772
Class "C" Concrete	C.Y.	7
THESE QUANTITIES NOT CORRECTED		
Deformed Steel Bars	Lb.	438,000
Structural Carbon Steel	Lb.	1,870,000
Structural Silicon Steel	Lb.	1,900,000
Open Steel Grid Floor	S.F.	7860
Damp Proofing	S.Y.	324
Metal Bridge Rail	L.F.	1568
Pervious Material	C.Y.	2430
Wire Rope Railing	L.F.	140
Type "C" Anchorage	Ea.	1
Compensating Type Anchorage	Ea.	1
Single Post	Ea.	2



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STANDARD CONVENTIONS

Hedge	Picket Fence	Present Culvert
Guide Rail	Board Fence	Proposed Culvert
Curb	Retaining Wall	House
Stone Wall	Open Ditch	Barn
Concrete Walk	Open Ditch	Buildings
Walk, for Gravel or Earth	Telegraph, Telephone, or Electric Light Poles	Swamp
Conn. Co.	Trolley Poles	Gas Random Pump Stone
Present Tracks	Ledge or Rock	Highway Man. "C.D. Marker"
Proposed Tracks	Tree Hydrant	Present C.B.
Taking Line	Pole	Proposed C.B.
Property Line	Rivers, Lakes, etc.	Present Travel Path
Town Line	North Point	
Grape Arbor		
Wire Fence		

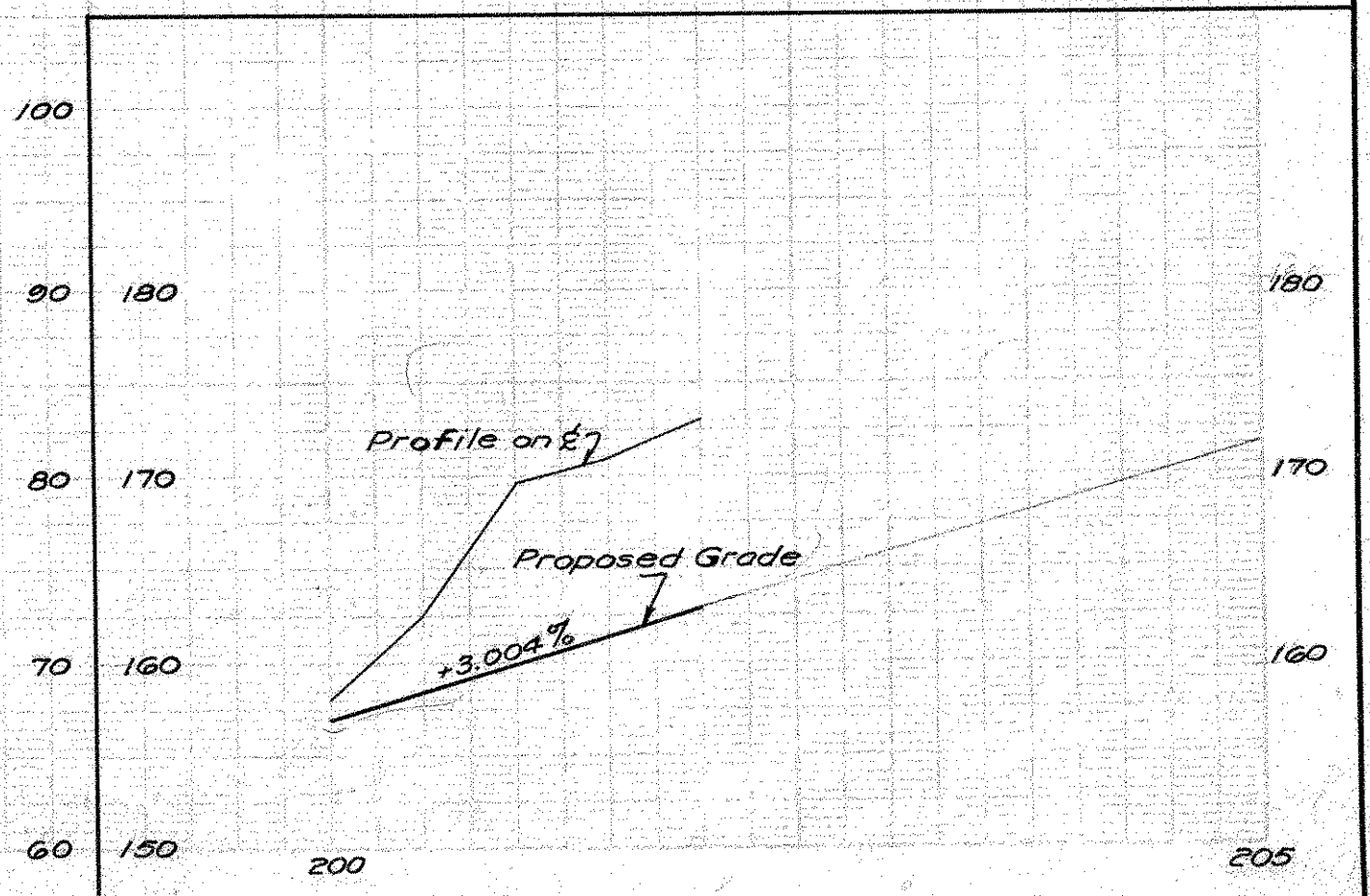
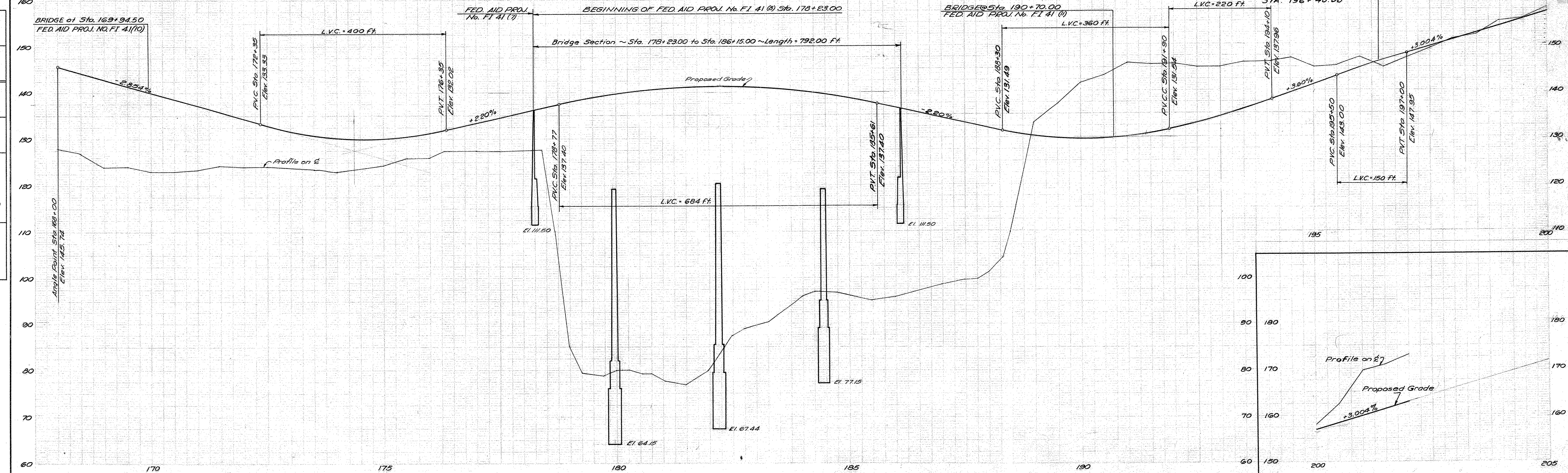
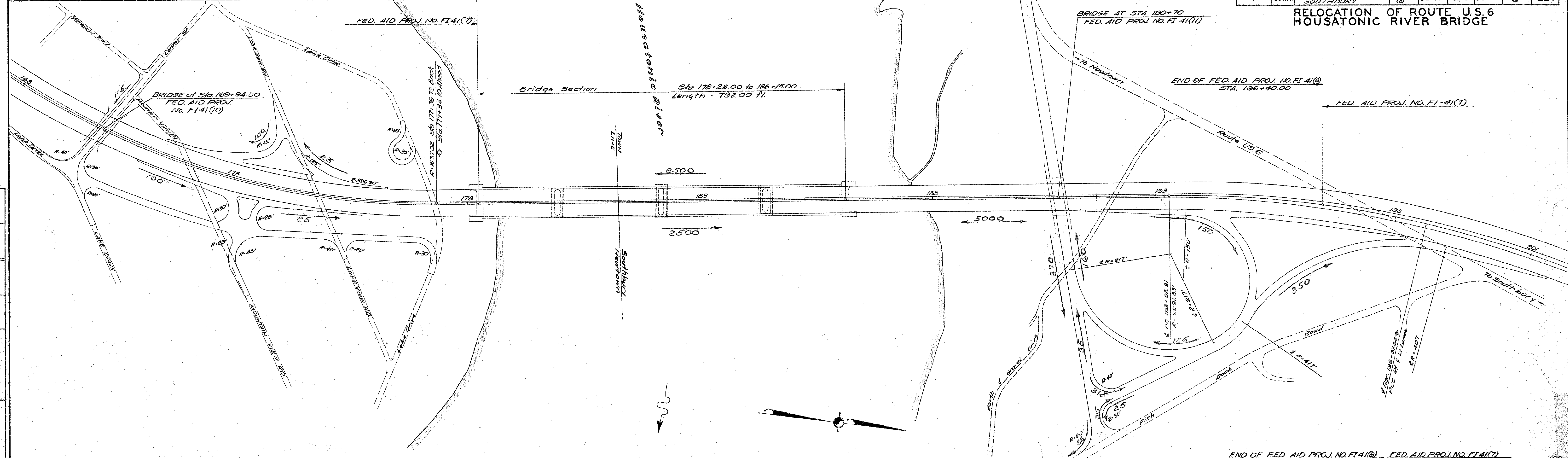
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INDEX PLAN

SCALE - 1"=100'

PUB. ROAD DIV. NO.	STATE	TOWN	FED. AID PROJ. NO.	PROJ. NO.	YEAR	ROUTE NO.	SHEET NO.	TOTAL SHEETS
1	CONN.	NEWTOWN & SOUTH BURY	FI-41 (2)	96-49	1990	US 6	2	25

RELOCATION OF ROUTE U.S. 6 HOUSATONIC RIVER BRIDGE



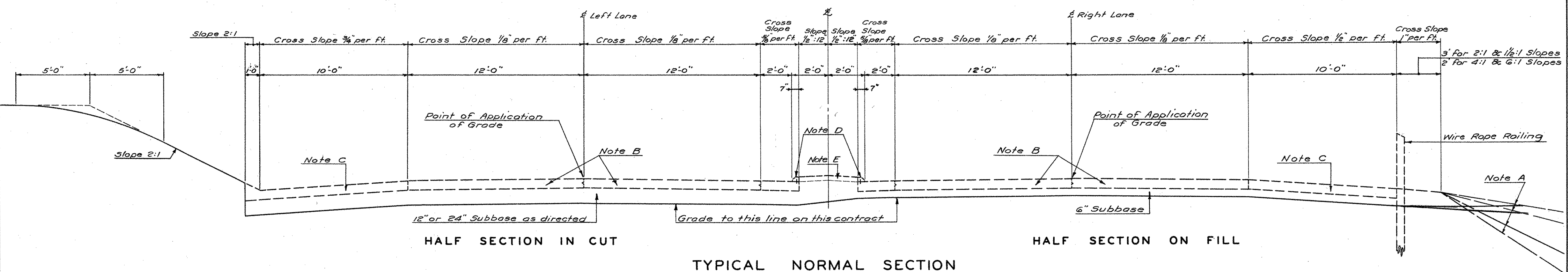
PLAN	NOTE BOOK NO.
Surveyed	
Plotted	
Alignment	
Traced	
New Const. Noted	
Designed	

PROFILE	NOTE BOOK NO.
Surveyed	
Plotted	
Traced	
New Const. Noted	
Designed	

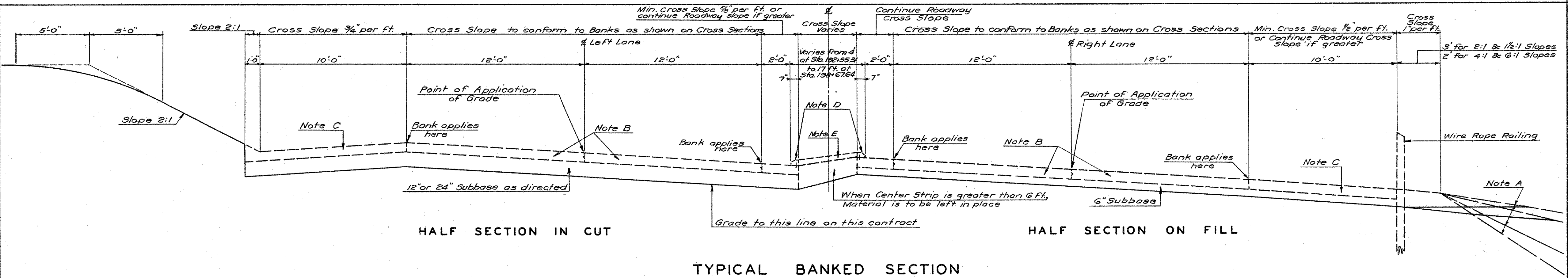
TYPICAL CROSS SECTIONS NOT TO SCALE

PUB. ROAD DIV. NO.	STATE	TOWN	FED. AID PROJ. NO.	PROJ. NO.	YEAR	ROUTE NO.	SHEET NO.	TOTAL SHEETS
1	CONN.	Newtown & Southbury	FI 41(B)	96-49	195	US 1	3	25

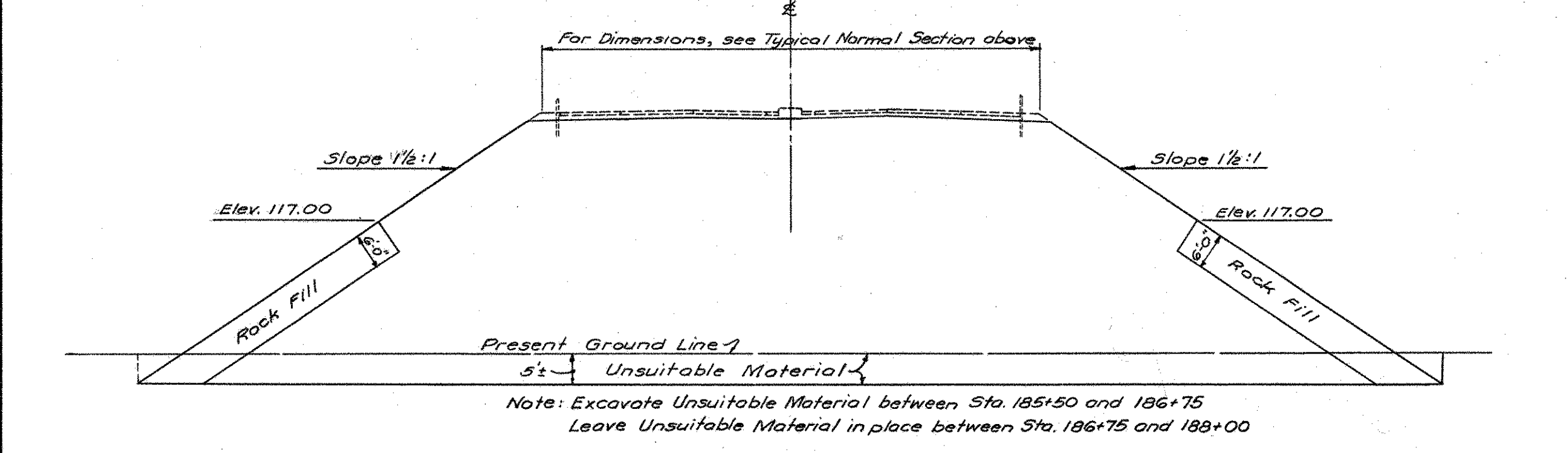
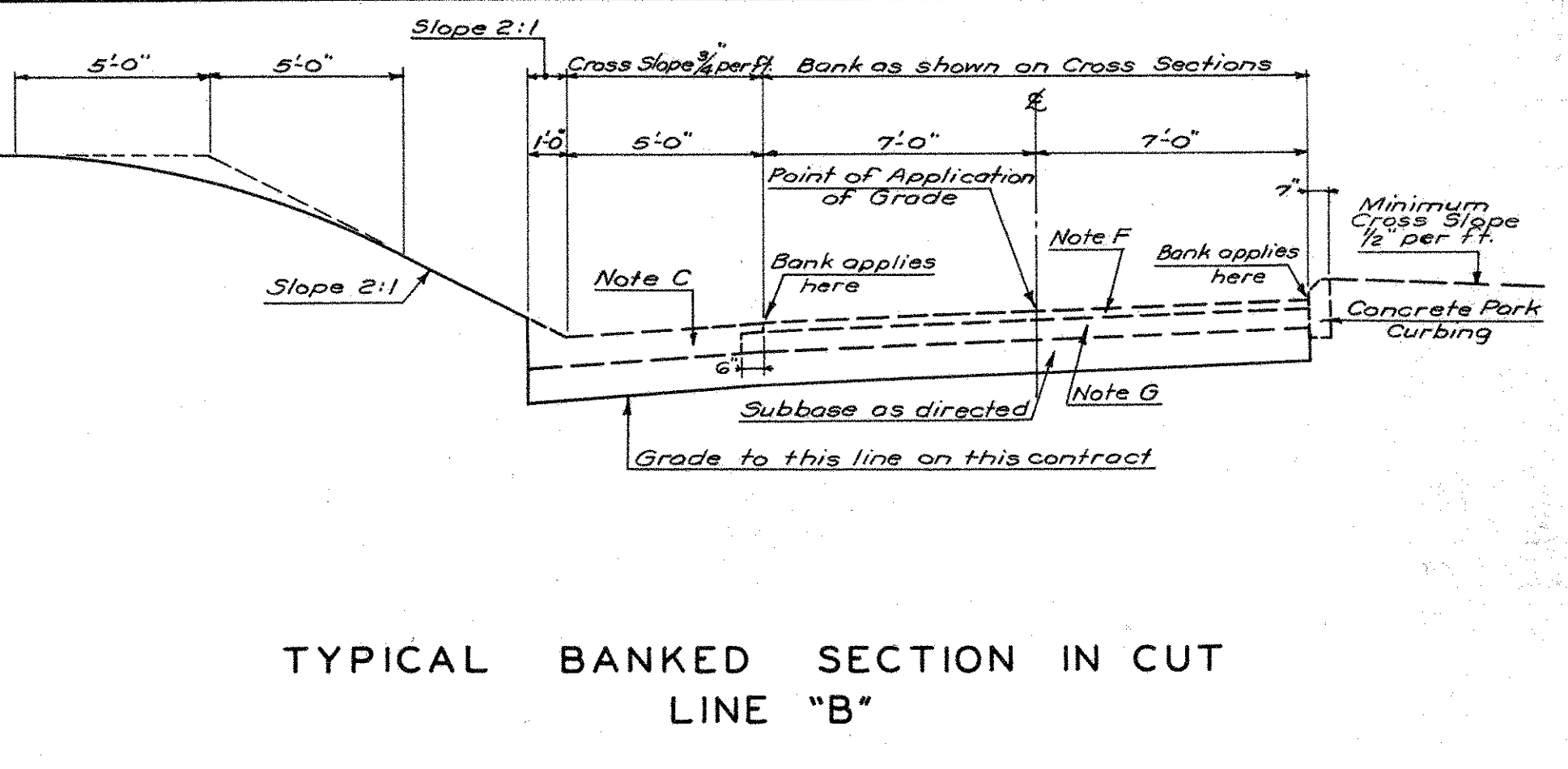
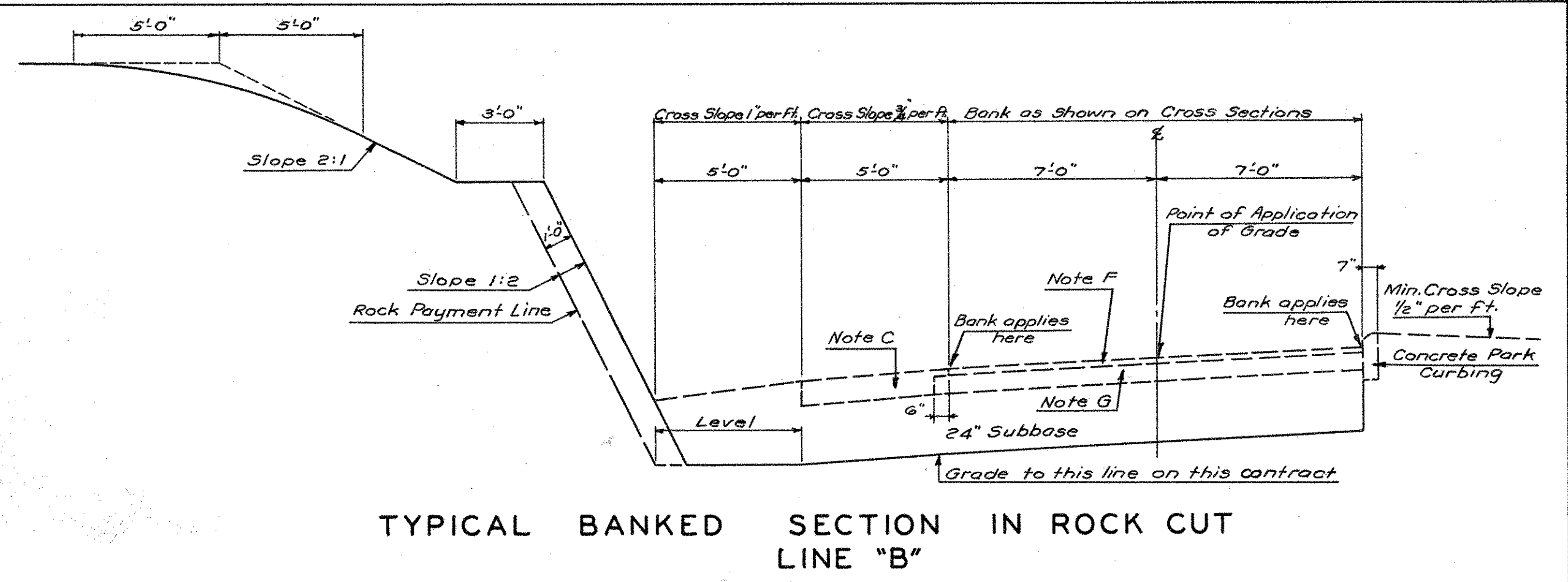
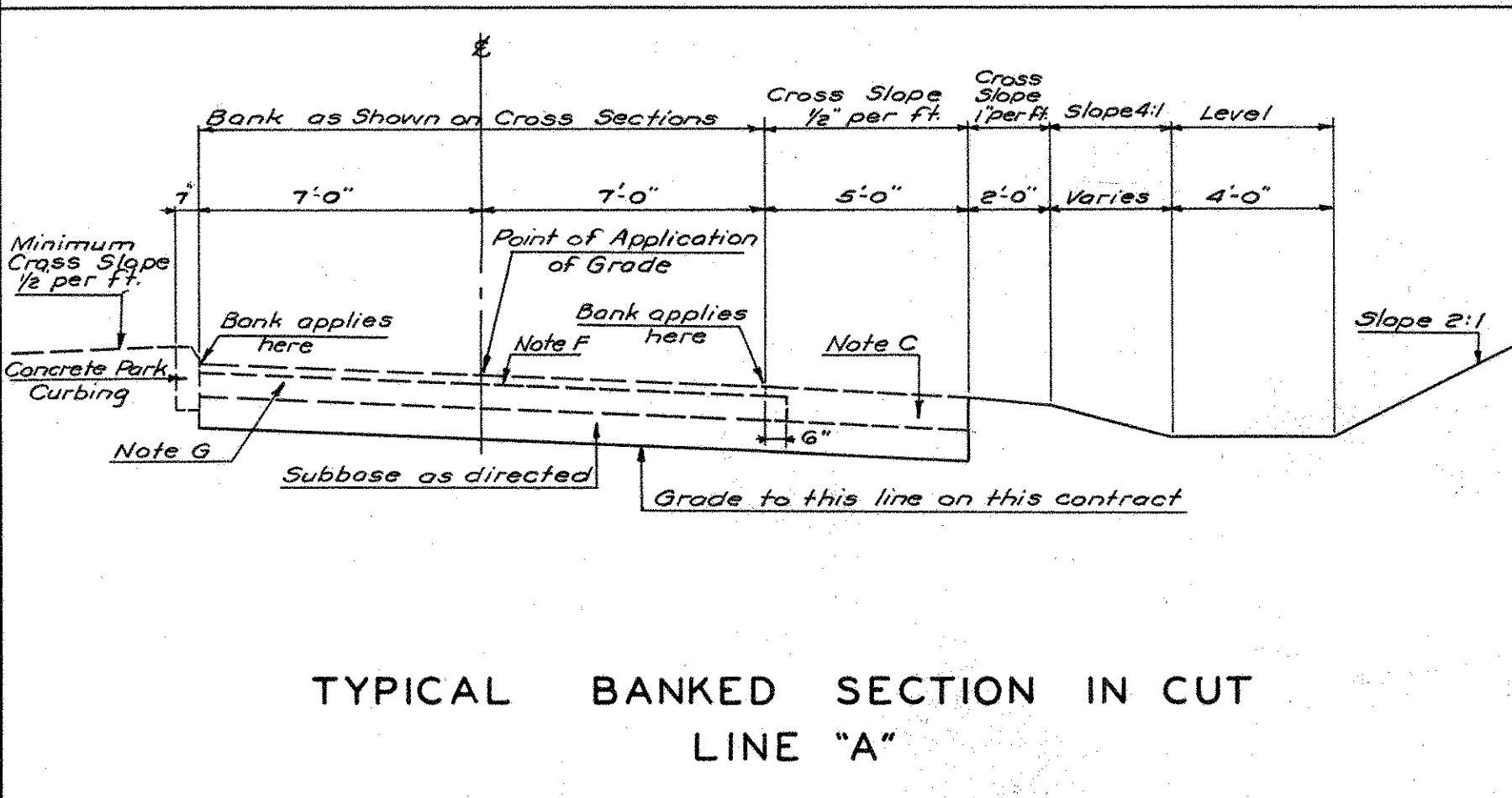
RELOCATION OF ROUTE US 6 - HOUSATONIC RIVER BRIDGE



- Note A: Slope 1 1/2 to 1 at Bridge.
Slope 2 to 1 for Fills over 8 ft.
Slope 4 to 1 for Fills of 4 ft. or less.
Slope 6 to 1 for Fills of 4 ft. to 8 ft.
- Note B: 8" Reinforced Concrete Pavement.
- Note C: Gravel Shoulder, 8" in depth after compaction.
Top Course: Bituminous Stabilized Shoulder, 3" in depth.
Base Course: Gravel Shoulder, 5" in depth.
- Note D: Doweled Concrete Park Curbing.
- Note E: 4" Loam by State Forces.
- Note F: 2 1/2" Bituminous Macadam Pavement.
- Note G: 6" Broken Stone Base, laid in two 3" courses.



PLAN	Surveyed	Checked by	Date
NOTE BOOK	Plotted	Smith	July 1950
NO	Traced	W. S. Bissell	Aug. 1950
	Designed	C. P. Bell	Aug. 1950
	New Const. noted		



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PUB. ROAD DIV. NO.	STATE	TOWN	FED. AID PROJ. NO.	FISCAL YEAR	ROUTE NO.	SHEET NO.	TOTAL SHEETS
1	CONN.	NEWTOWN SOUTHBURY	FI-41(8)	1950	US6	4	25

CONNECTICUT LIGHT & POWER CO.

FED. AID PROJ. NO. FI 41 (7)

BEGINNING OF FED. AID PROJ. NO. FI 41 (8)
STA. 178+23.00

B.M. No. 21 - Elev. 128.46
R.R. Spike in Tree

15' Oak
HOWARD B. CORSA

Original
PT. 177115.69
PT. Base Line 177156.73
177134.00
PT. Lt. Lane 177156.87
Base Line Sta.

To Newtown
177
178
179
180
181
182
183
184
185
186
187
188
189

177
178
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189

PT. Rt. Lane 177114.98
Base Line Sta.

Highway Line
RIVER SIDE ON LAKE ZOAIR
ASSOCIATION, INC.

Garage
PHILIP & HELEN
ZIEGLER MILLER

Garage
Top of Slope

Garage
Top of Slope

Garage
Top of Slope

Garage
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Garage
Top of Slope

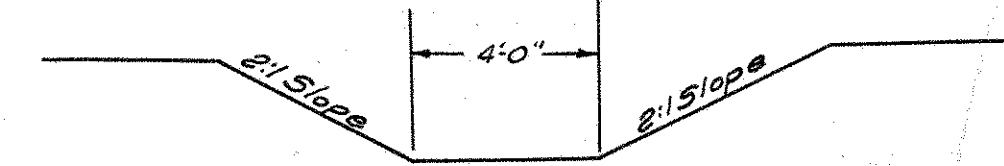
HOUSATONIC

NEWTOWN SOUTHBURY

TOWN LINE

RIVER

Excavation to be paid for as "Earth Excavation"



Cross Section of Channel
Lt. of Sta. 183+05

RELOCATION OF ROUTE U.S. 6
Minimum Width of Right of Way - 170 Ft.
HOUSATONIC RIVER BRIDGE

BLDG. OWNED BY
CHARLES
PHILEAS
ANGLERO
1908

THE CONNECTICUT
LIGHT & POWER CO.

20' Oak
12' Ash
12' Ash
WILLIAM G. &
ALICE E.
SCHIELKE

18' Apple
18' Apple

18' Apple

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20' Oak
12' Ash
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WILLIAM G. &
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18' Apple

Rights of Access
Acquired

EL. 103.0
C.L. & P. Co.
Datum

B.M. No. 22 - Elev. 124.06
R.R. Spike in Tree

Garage

Garage

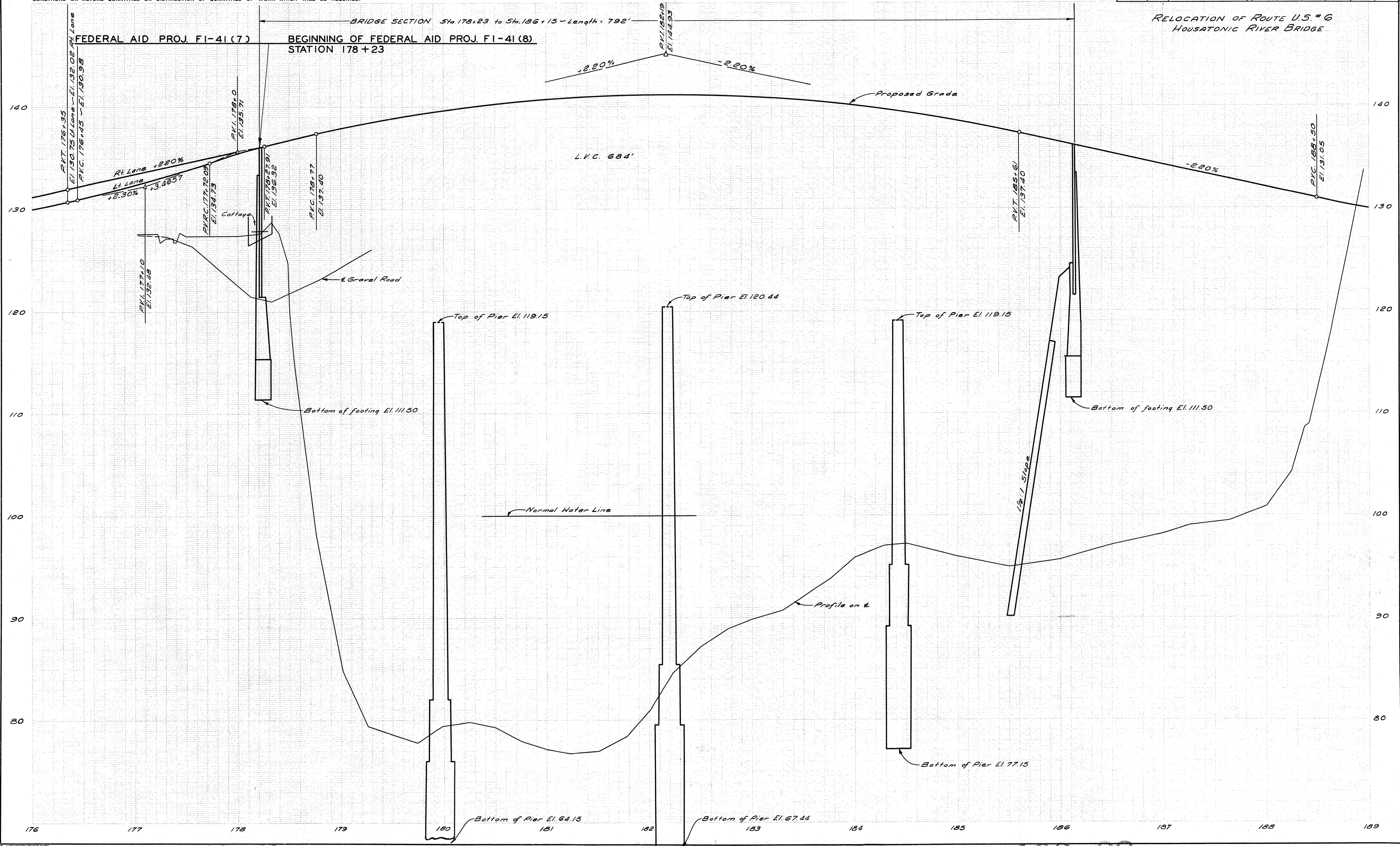
Garage

Garage

PLAN	BY	DATE	CREATED BY	DATE
Surveyed	LODD	1943		
Plotted	Reading	5-12-45		
Traced				
Designed				
New Const. noted				

PUB. ROAD DIV. NO.	STATE	TOWN	FED. AID PROJ. NO.	PROJ. NO.	YEAR	ROUTE NO.	SHEET NO.	TOTAL SHEETS
1	CONN.	Newtown-Southbury	F1-41(8)	96-49	1950	U.S. 6	5	25

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PROFILE	Lead	Year	Surveyed	Plotted	Traced	New Const. Noted	Designed
		1943					
		"					
		1950					
		"					
		1949					

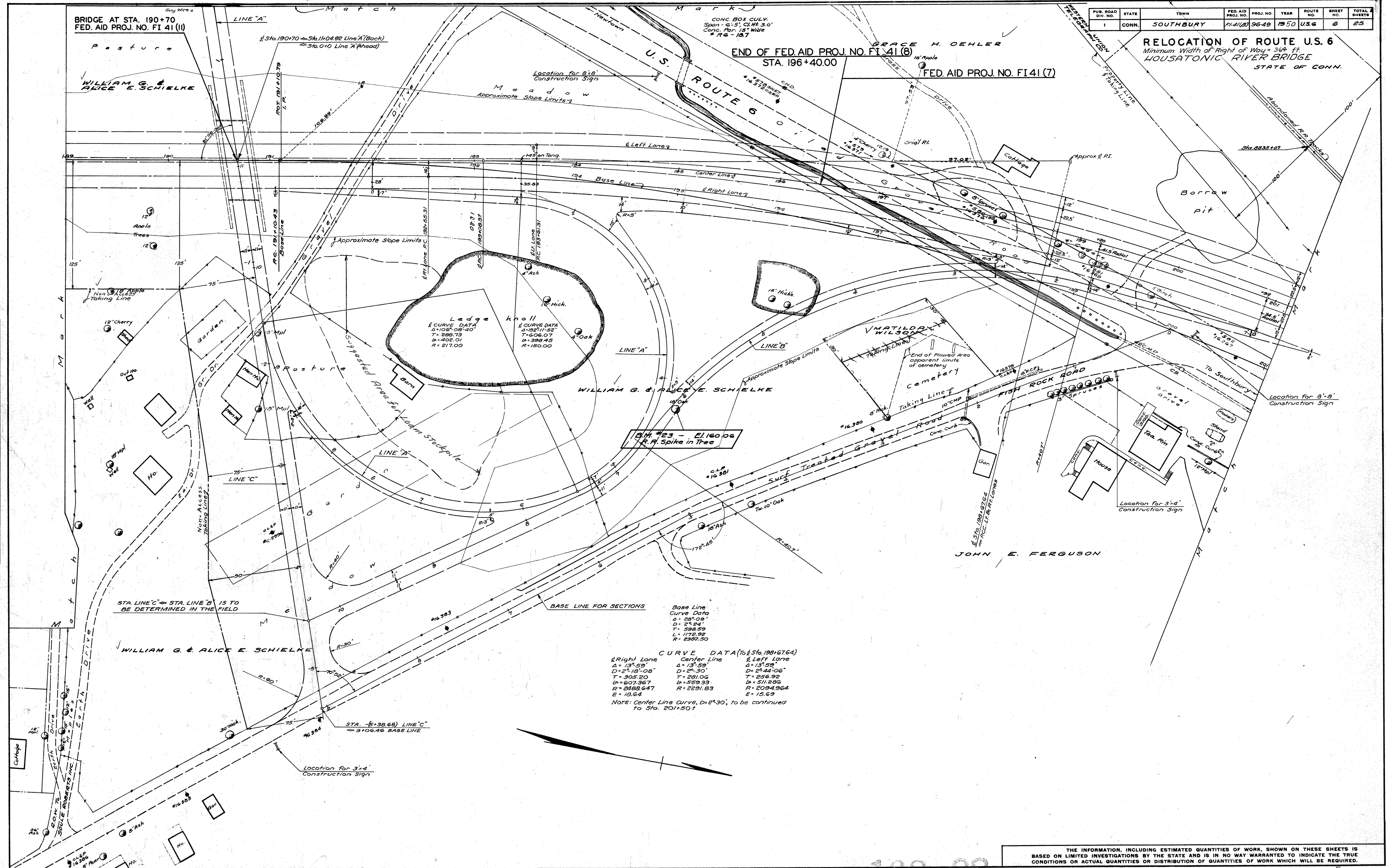
PUB. ROAD DIV. NO.	STATE	TOWN	FED. AID PROJ. NO.	PROJ. NO.	YEAR	ROUTE NO.	SHEET NO.	TOTAL SHEETS
1	CONN.	SOUTHBURY	F-141(8)	96-49	1950	U.S. 6	6	25

BRIDGE AT STA. 190+70
FED. AID PROJ. NO. FI 41 (II)

END OF FED. AID PROJ. NO. FI 41 (8)
STA. 196+40.00

RELOCATION OF ROUTE U.S. 6
Minimum Width of Right of Way = 364 ft.
HOUSATONIC RIVER BRIDGE
STATE OF CONN.

PLAN	DATE	BY	CHECKED BY
Surveyed	12/23	W.C.	
Plotted			
Traced			
Designed			
New Const. noted			



Base Line
Curve Data
Δ = 28° 08'
D = 2' 24"
T = 398.59
L = 1172.92
R = 2382.50

CURVE DATA (to Sta. 198+67.64)

Right Lane	Center Line	Left Lane
Δ = 13° 59'	Δ = 13° 59'	Δ = 13° 59'
D = 2' 18" 08"	D = 2' 30"	D = 2' 44" 06"
T = 305.20	T = 281.06	T = 256.92
l = 607.367	l = 559.33	l = 511.299
R = 2488.647	R = 2291.83	R = 2094.964
E = 18.64	E = 15.69	E = 15.69

NOTE: Center Line Curve, Δ = 2° 30', to be continued to Sta. 201+50±

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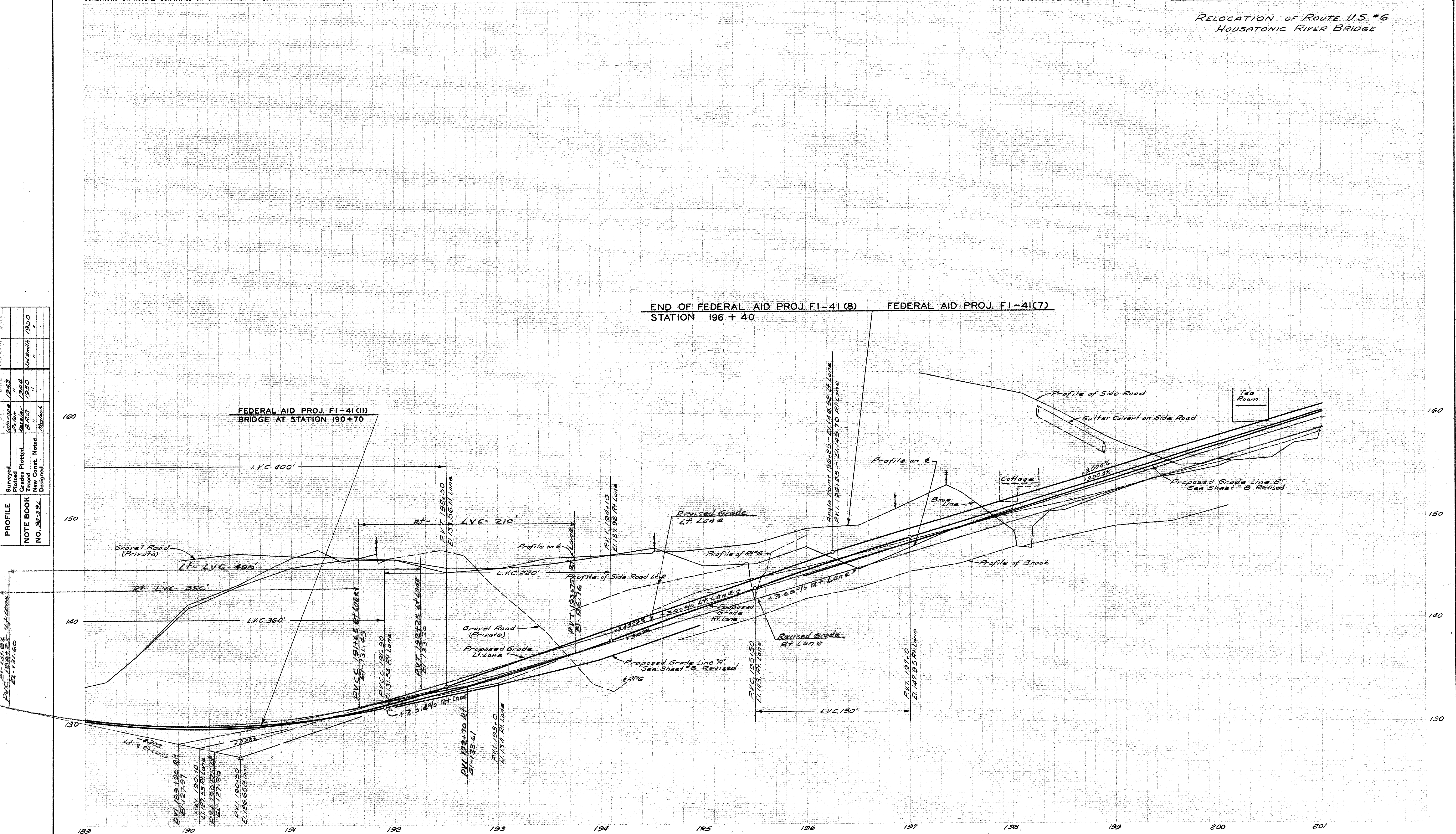
PUB. ROAD DIV. NO.	STATE	TOWN	FED. AID PROJ. NO.	PROJ. NO.	YEAR	ROUTE NO.	SHEET NO.	TOTAL SHEETS
1	CONN.	Southbury	FM(1)(B)	96-49	1950	U.S. 6	7	25

RELOCATION OF ROUTE U.S. #6
HOUSATONIC RIVER BRIDGE

PROFILE	NO.	DATE	BY	REVISIONS
Surveyed	1243			
Plotted	1243			
Traced	1243			
New Const. Noted	1243			
Designed	1243			

END OF FEDERAL AID PROJ. F1-41(8)
STATION 196 + 40

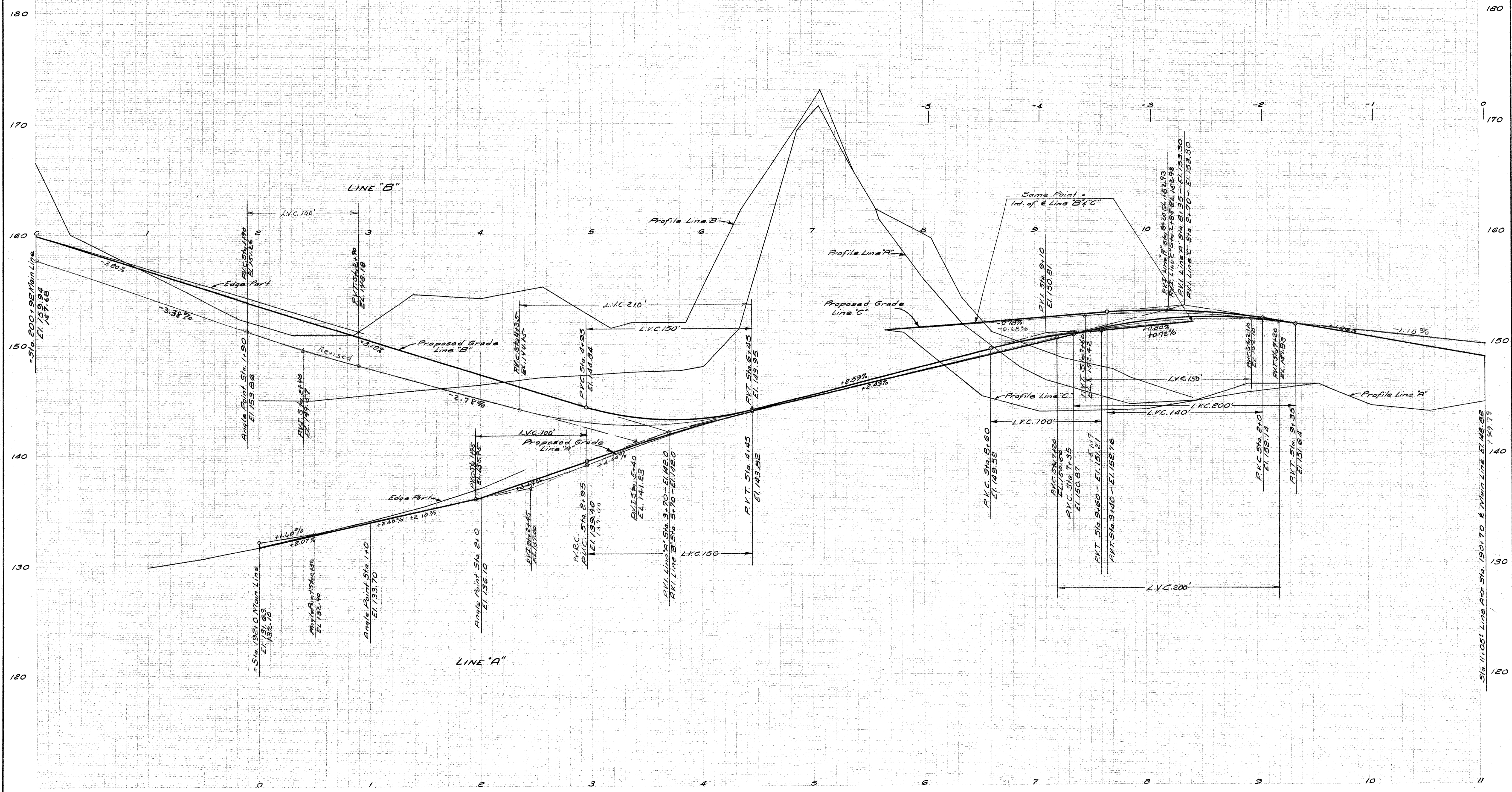
FEDERAL AID PROJ. F1-41(7)



PUB. ROAD DIV. NO.	STATE	TOWN	FED. AID PROJ. NO.	PROJ. NO.	YEAR	ROUTE NO.	SHEET NO.	TOTAL SHEETS
1	CONN.	Southbury	F-416	96-49	1950	U.S. 6	8	25

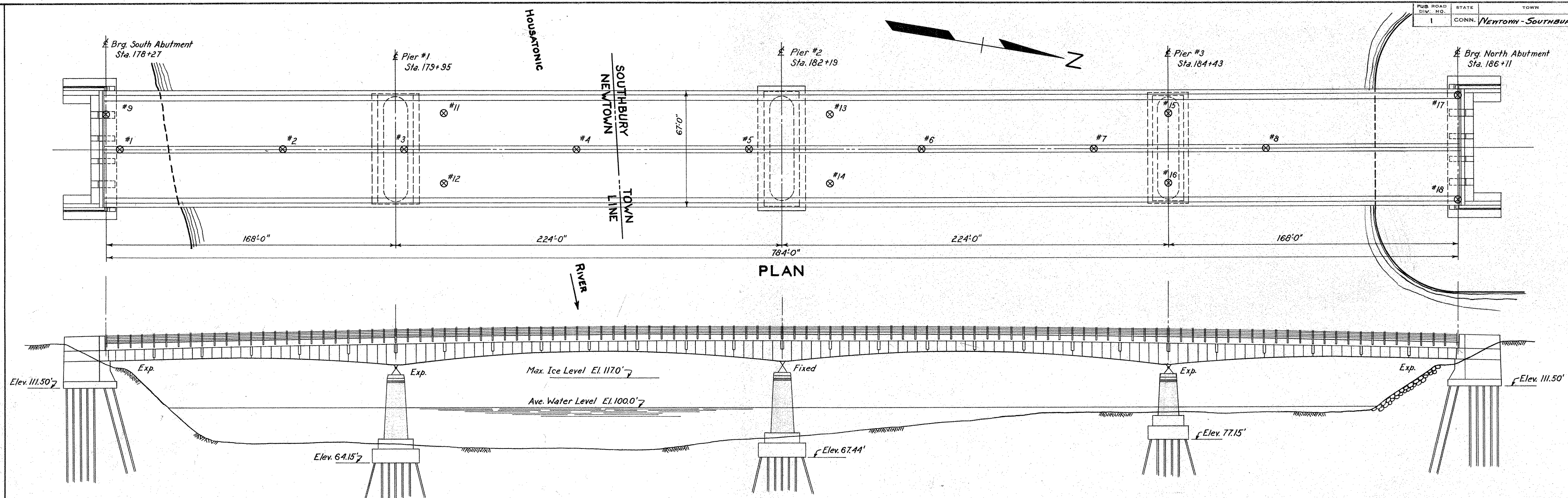
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RELOCATION OF ROUTE U.S. 6
HOUSATONIC RIVER BRIDGE
LINE "A" STA. 192+0 RT.
LINE "B" STA. 200+92 RT.



DATE	BY	DESCRIPTION
1950	W.A.S.	Surveyed
1950	W.A.S.	Plotted
	W.A.S.	Grades Plotted
	W.A.S.	Profile
	W.A.S.	New Const. Noted
	W.A.S.	Designed

NOTE BOOK NO. 96-39-0



ELEVATION

GENERAL NOTES

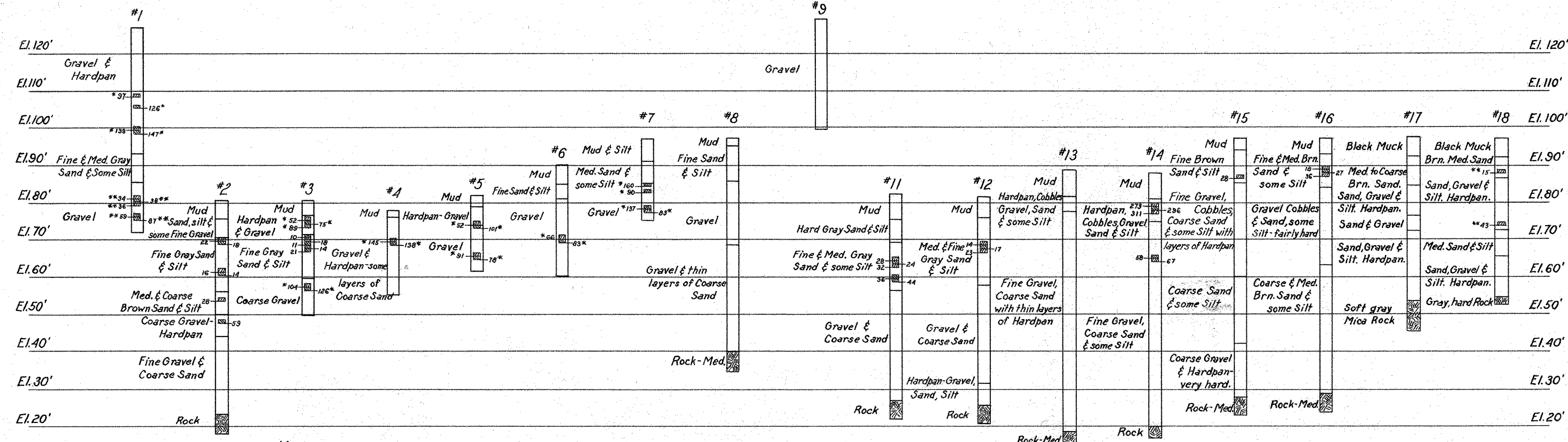
Specifications - Conn. State Highway Dept. Form 807, 1947 and AASHO H20-S16-49 and Special Provisions.
 Structural Steel and Metal Bridge Rail to be painted one shop coat of Zinc Chromate and Iron Oxide and two field coats of Aluminum. See Special Provisions. No paint to be used on those surfaces in contact with the concrete.
 All necessary welding and weld material to be included in the item of Structural Steel.
 Class A Concrete to be used thruout except as noted for grid flooring. Deformed Bars shall be of an approved type.
 Quantities are approximate only and do not relieve the Contractor of the responsibility of checking them in preparing his bid.

DISTRIBUTION OF CLASS A CONCRETE

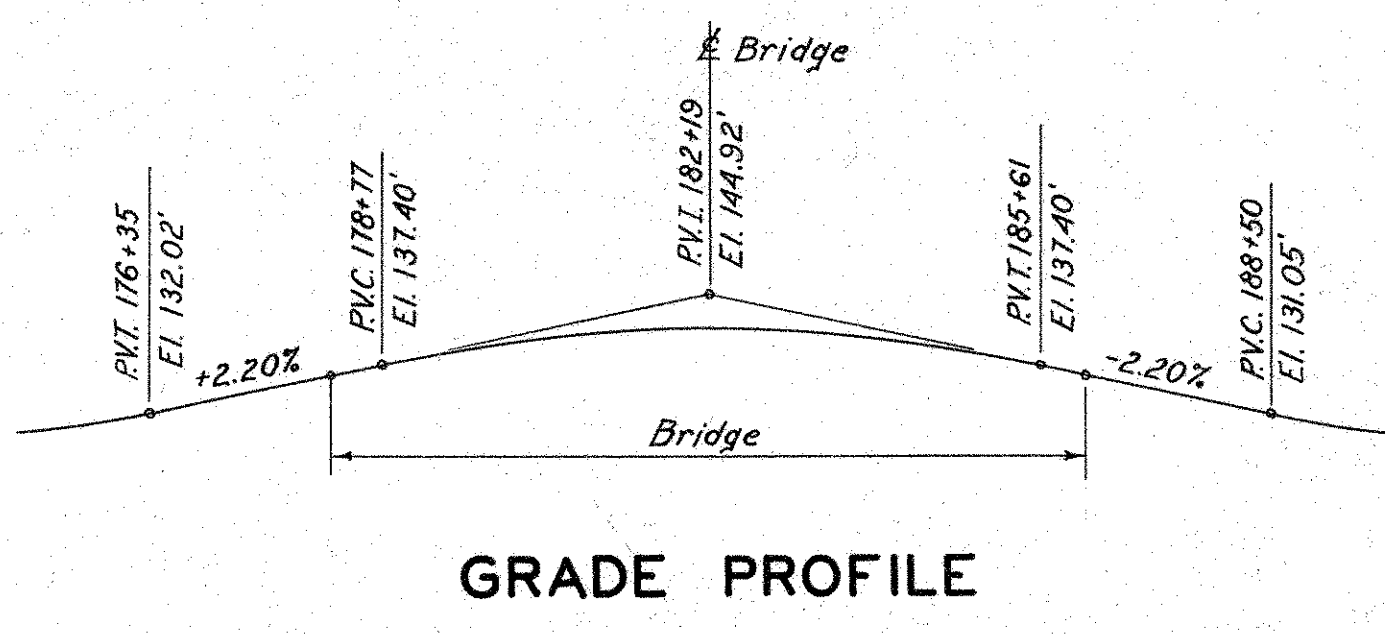
FOOTING	SUBSTRUCTURE	SUPERSTRUCTURE	TOTAL
2850 c.y.	2800 c.y.	1028 c.y.	6681 c.y.

TREMIE CONCRETE 772 c.y.

BORINGS



NOTE:
 The number opposite the strata is the number of blows needed to drive the Sampler Pipe 1'-0" with a 300 lb. hammer falling 2'-0" & using a 3" diameter Pipe.
 * denotes 4" casing Pipe used.
 ** denotes 2 1/2" casing Pipe used.

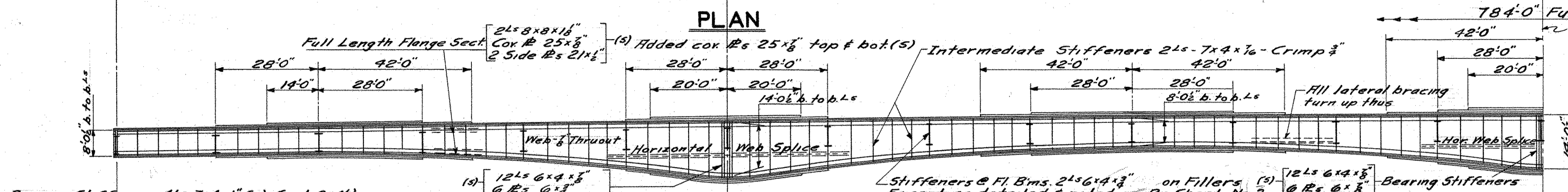
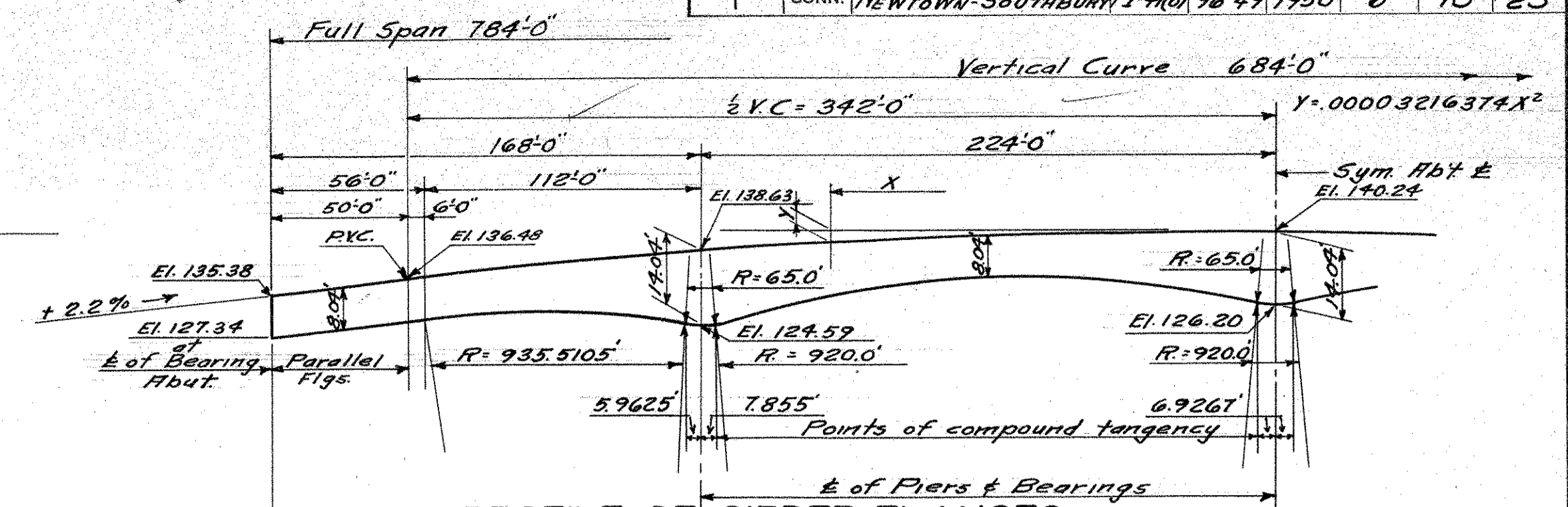
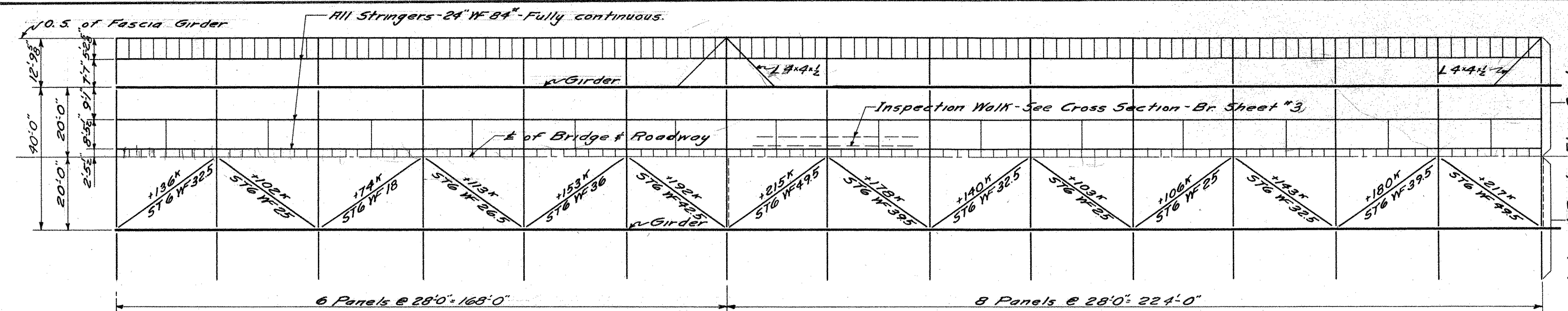


GRADE PROFILE

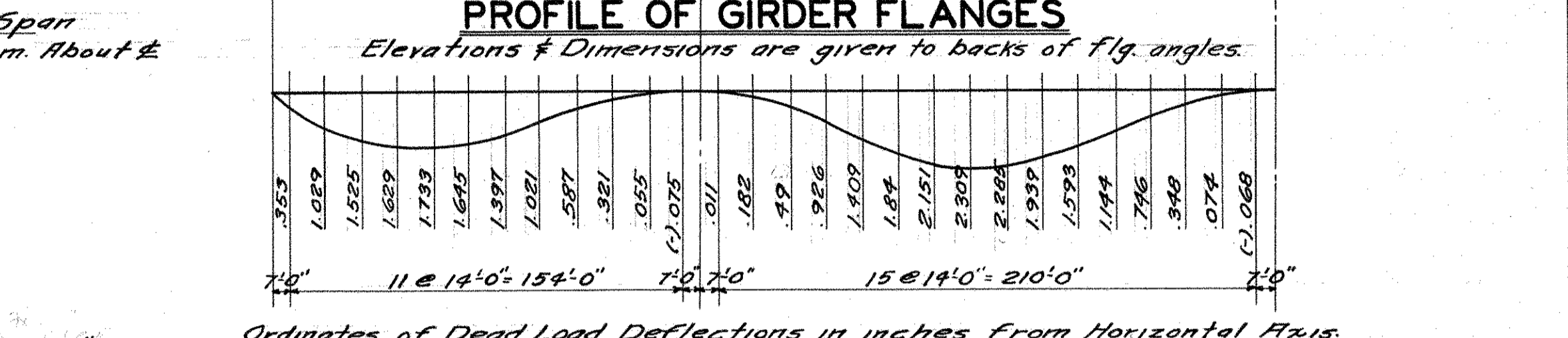
Fed. Aid Proj. FI-41(8)
 CONNECTICUT STATE HIGHWAY DEPARTMENT
 HOUSATONIC RIVER BRIDGE
 RELOCATION U.S. ROUTE NO. 6
 BETWEEN
 NEWTOWN AND SOUTHBURY
 GENERAL PLAN-PROFILE

REVISIONS		
NO.	DATE	DESCRIPTION

DESIGNED BY F.J.T.	PROJECT NO.
SCALES 1"=30'-0"	96-49-03
MADE BY V.J.B.	DATE 6/27/50
CHECKED BY F.J.T.	DATE 7/24/50
APPROVED J.D. Drury	DATE 7-28-50
BRIDGE SHEET NO. 1	OF 9

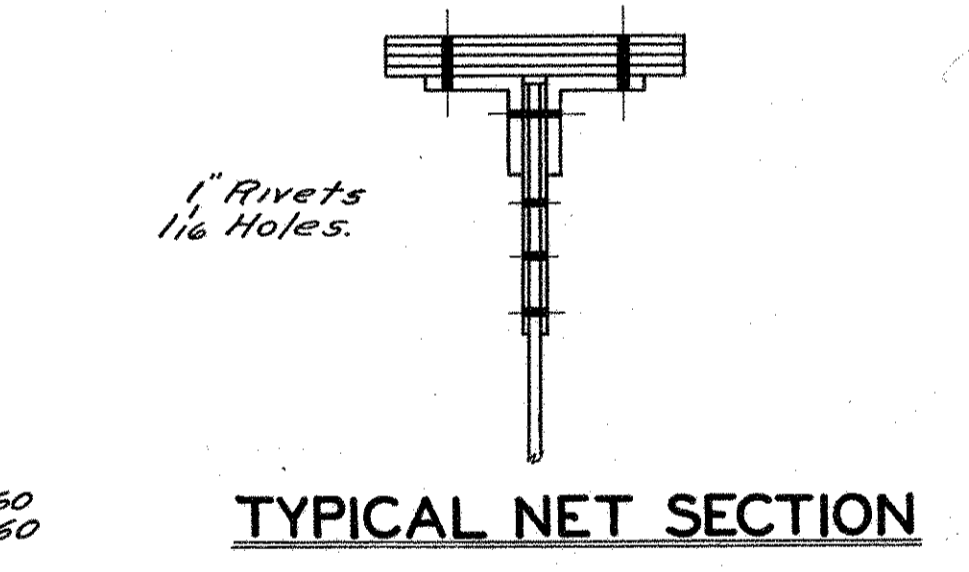
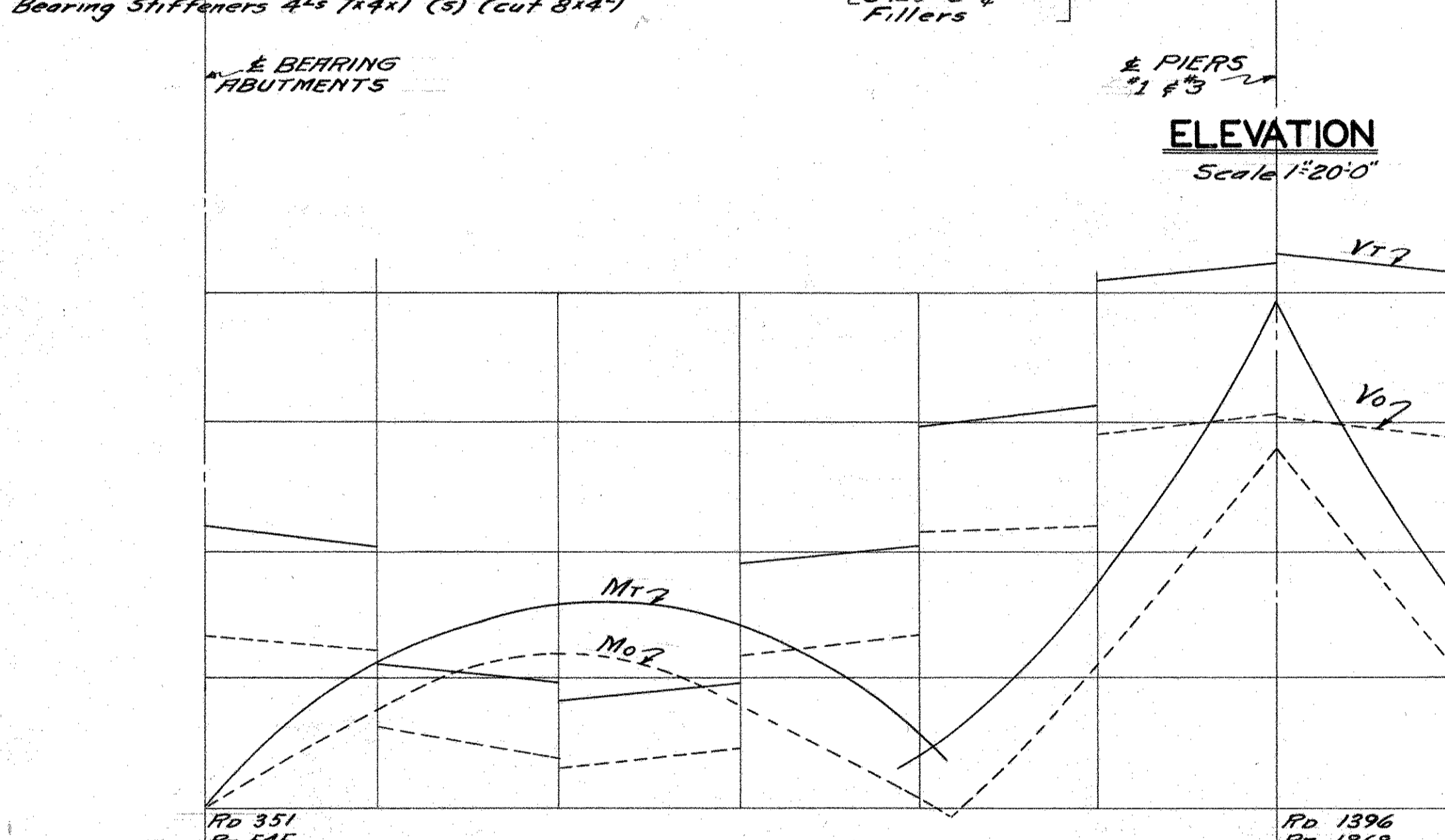


NOTES: All main material in Girders to be Silicon Steel. (5)
 Horizontal web splices to meet with the approval of the Department & Vertical Web splices and flange splices to meet with the approval of the Department and will not be permitted at points of support.
 Top Flange of Girder is to be parallel to grade of Rdwy. See profile
 Bott. Flange of Girder to be made to tangent slope & radii shown
 Rivets 1" - Holes 1 1/8" in Girder.
 Rivets 5/8" - Holes 1/2" - Otherwise.

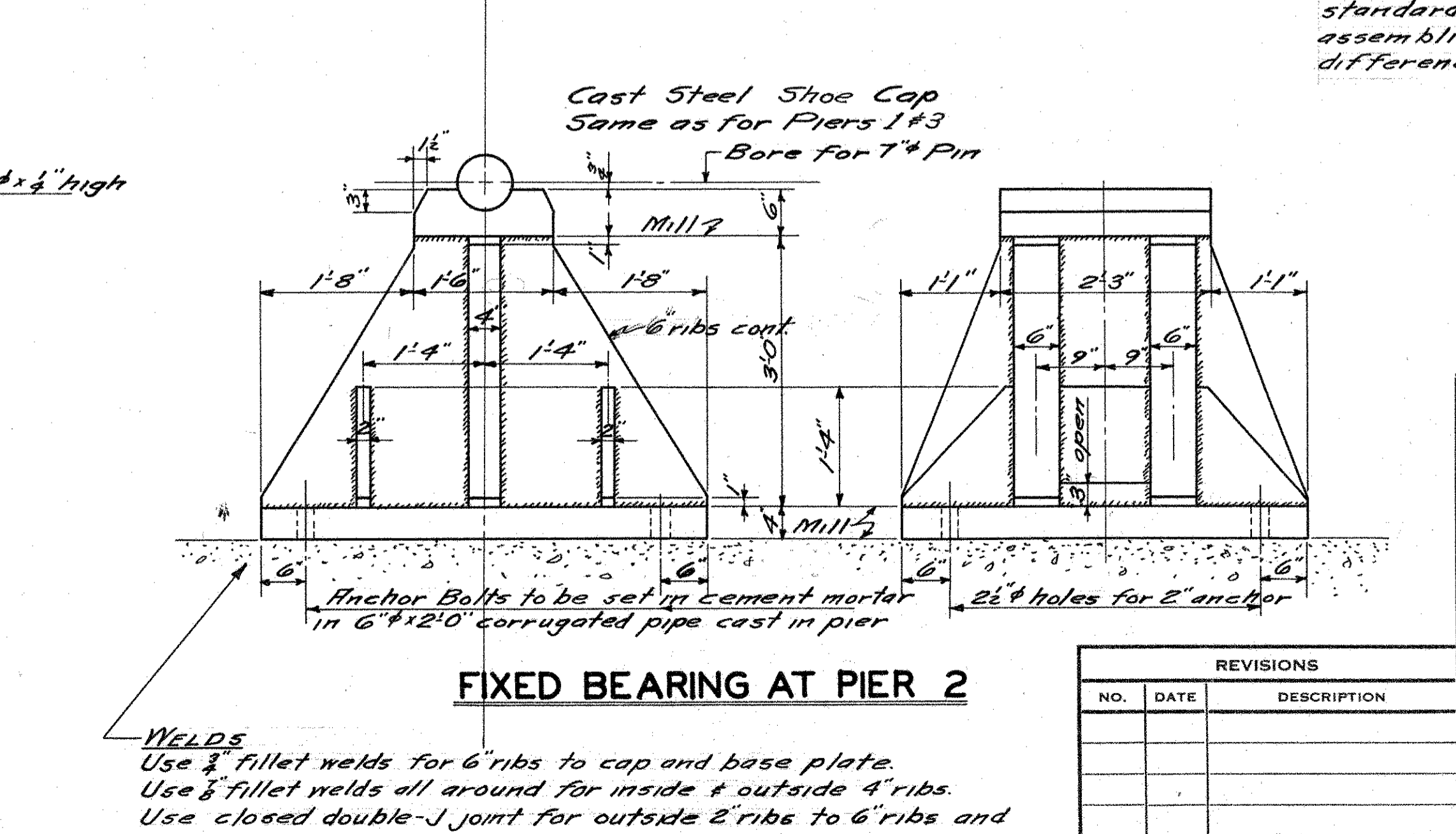
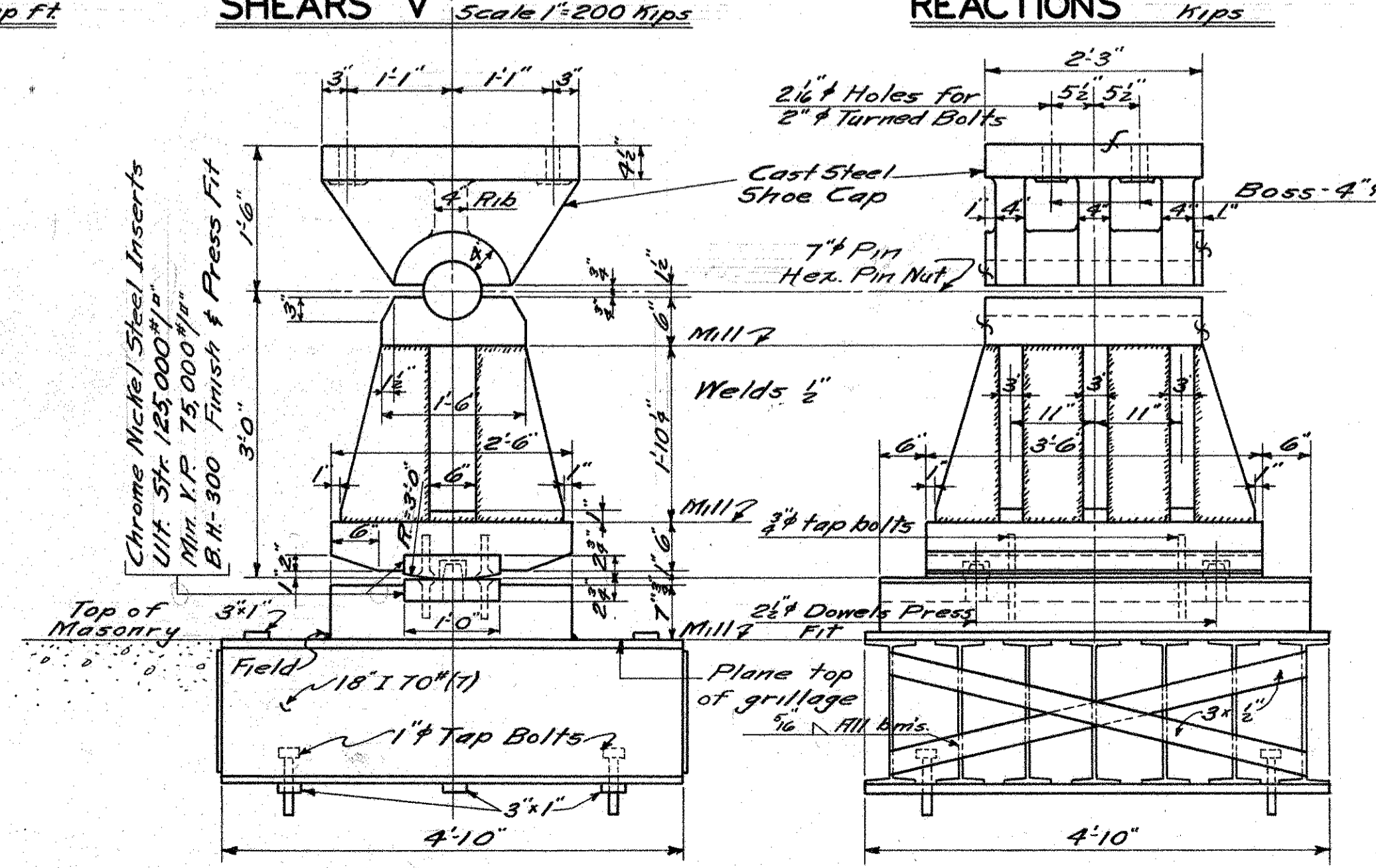
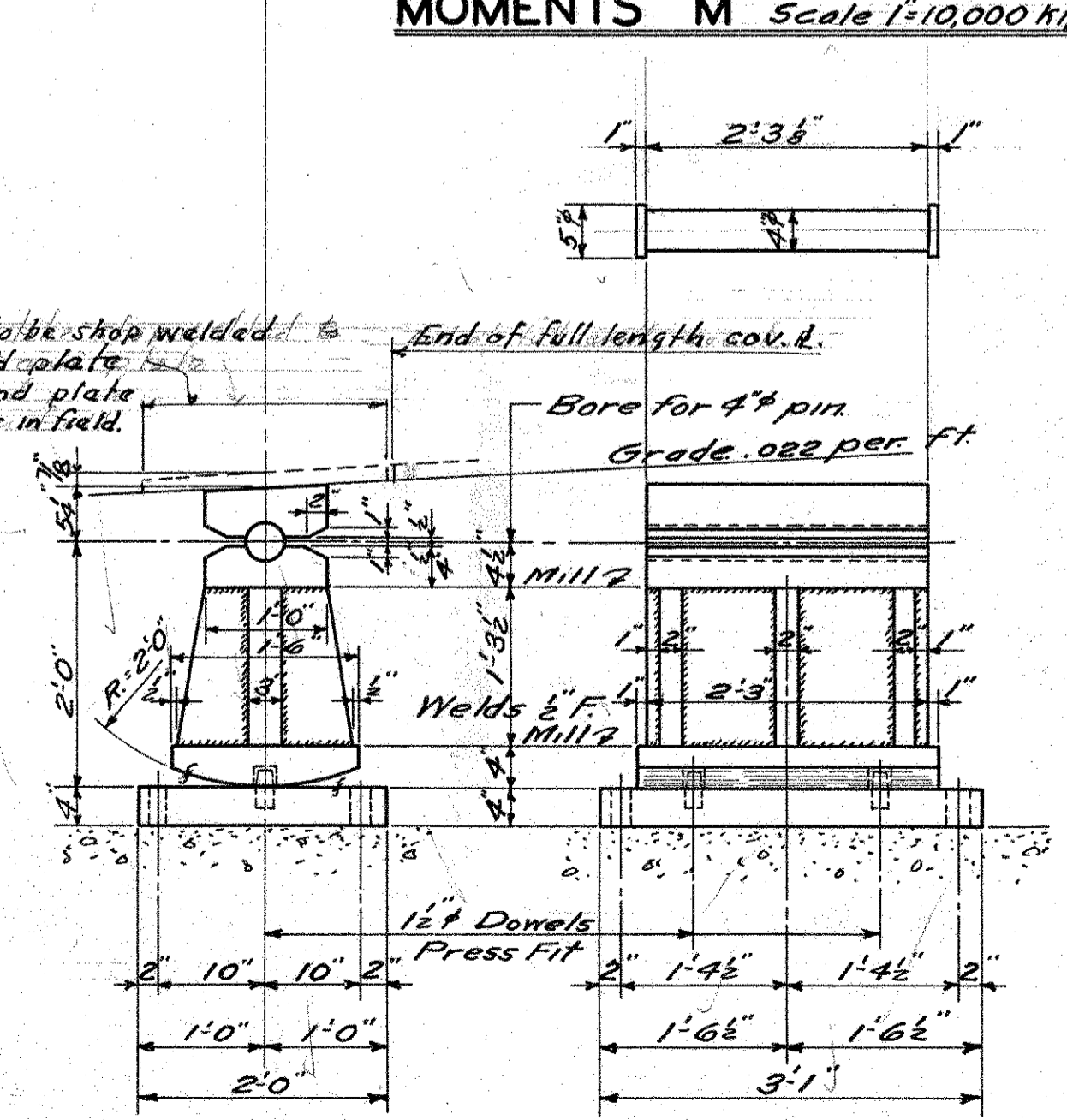


GIRDER DATA

STRINGERS	FLOOR BEAM	BRACKET	TENSION PLATE
M(1) 710	M(1) 886	M(1) 428	M(1) = 1043 k.f.
Dead Load 184.5	148.5	87.7	T = C = 226 k = 1043 ÷ 4.62
Live Load 31.5	25.2	24.1	Net Area Req'd = 12.56 sq"
Impact 263	24.1	126	Use 14 x 1/8" PL
Total 287 k.f.	262 k.f.	1568 k.f.	Net Area = 13.5 sq"
Req'd 5"	1912 ³	1046 ³	
	24 W 84-196.3	4 1/2 x 6 x 1/2	
		Web R-66 x 3/8	
		5-1170	
		696 ³	
		4 1/2 x 6 x 1/2	
		Web R-66 x 3/8	
		5-790	



NOTE:
 Design details & stresses for Girders shown here.
 All splices, shop and field to be determined by the Fabricator and submitted for approval.
 All floor beams, brackets and stiffeners to be vertical.
 Detailed lengths of girders and stringers shall include allowance for the difference between the 55° normal temperature of the structure and the standardization temperature of all tapes used. Standardized field tapes employed in laying out the substructure shall be checked with standardized shop tapes used in checking shop assemblies, compensation shall be made for all differences.



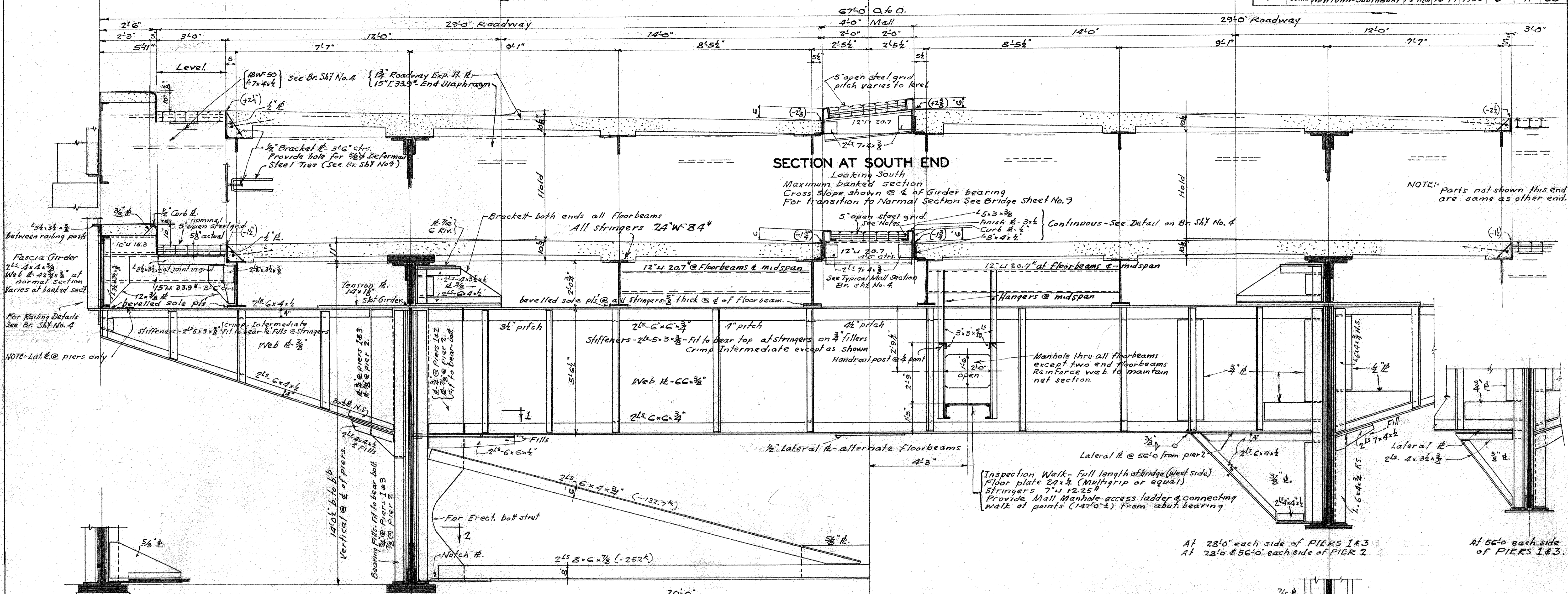
Fed. Aid Proj. FI-41(8)

CONNECTICUT STATE HIGHWAY DEPARTMENT
HOUSATONIC RIVER BRIDGE
RELOCATION U.S. ROUTE NO. 6
BETWEEN
NEWTOWN AND SOUTHURY
GIRDERS & PEDESTALS

DESIGNED BY F.J.T.
 SCALES 3/4" = 1'-0" Unless noted
 MADE BY H.W.M. Tr. by W.F.D. DATE 6/19/50
 CHECKED BY F.J.T. DATE 7/1/50
 APPROVED J.D. DUNN DATE 7/24/50

PROJECT NO. 96-49-03
 BRIDGE SHEET NO. 2 OF 9

Point of application of Grade & Cross Slope



NOTE: Parts not shown this end are same as other end.

Inspection Walk - Full length of bridge (West side)
Floor plate 24x4 (Multygrip or equal)
Stringers 7x12.25"
Provide Mall Manhole access ladder & connecting walk at points (147'-2") from abut. bearing

At 28'-0" each side of PIERS 1 & 3
At 28'-0" & 56'-0" each side of PIER 2

NOTES

All floorbeams & brackets are to be level under full dead load.
Webs of floorbeams & brackets are to be vertical.
Rivets - 1" Holes 1/8" in Girders
Rivets - 7/8" Holes - 1/16" otherwise
5" open steel grid in Mall to be filled with concrete 15' each side of all piers and 10' at each abutment.

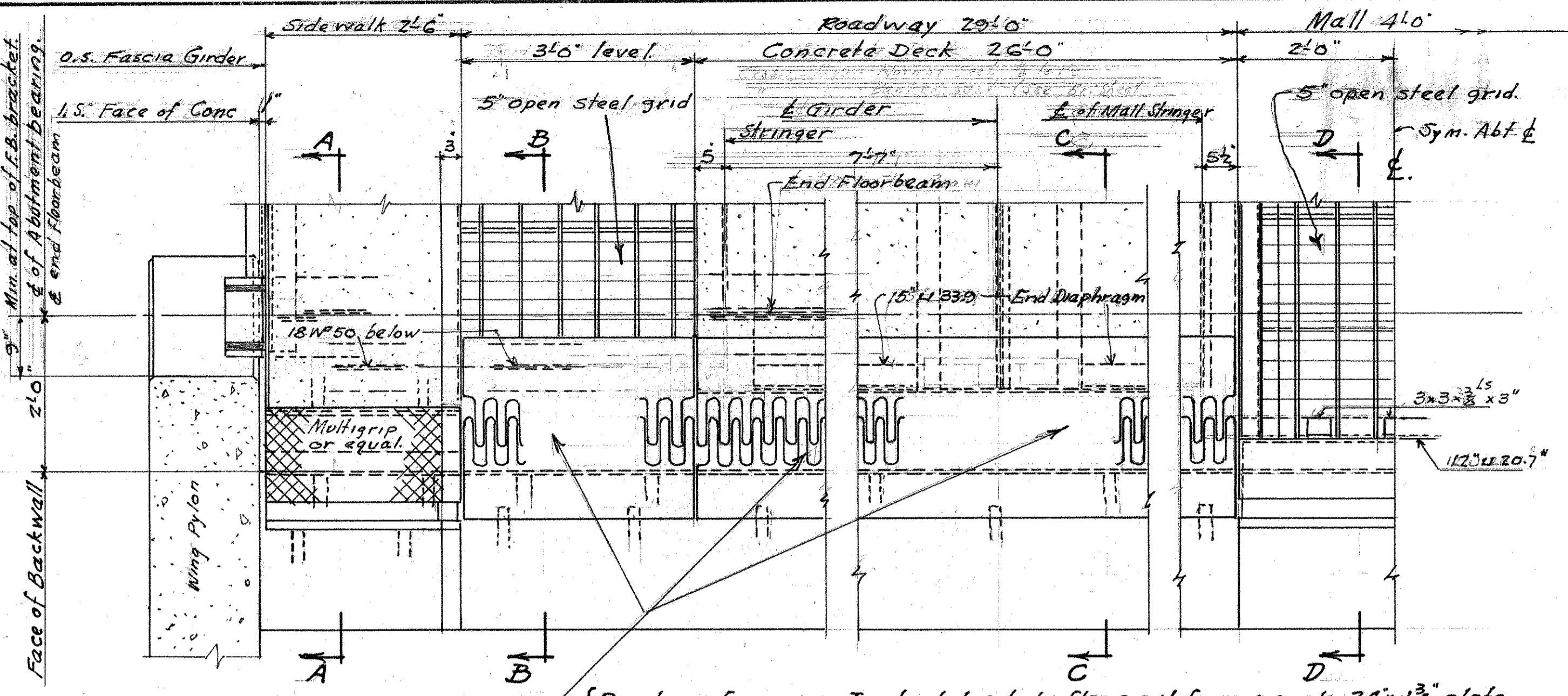
REVISIONS		
NO.	DATE	DESCRIPTION

Fed. Aid Proj. FI-41(8)

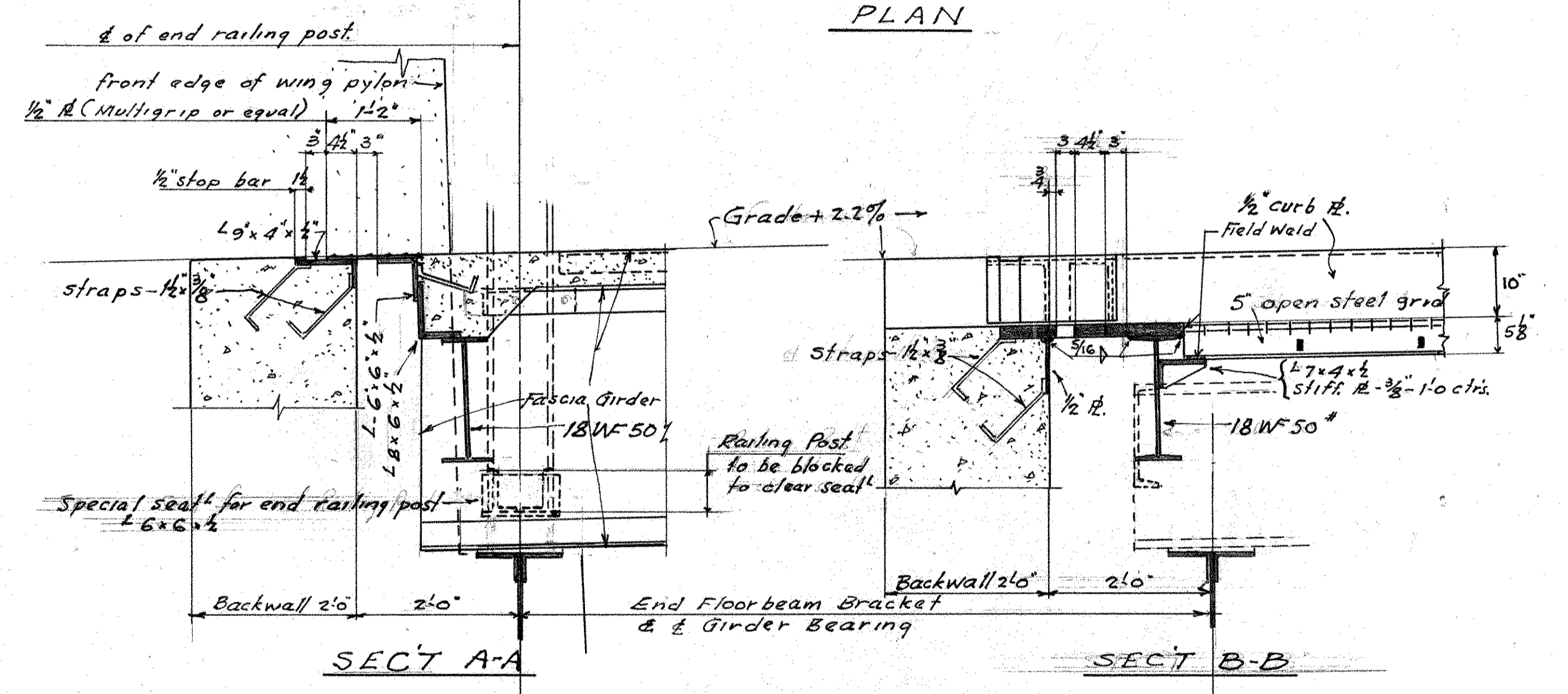
CONNECTICUT
STATE HIGHWAY DEPARTMENT
HOUSATONIC RIVER BRIDGE
RELOCATION U.S. ROUTE NO. 6
BETWEEN
NEWTOWN AND SOUTHBURY
TYPICAL CROSS SECTIONS

DESIGNED BY F.J.T.
SCALES 1/2" = 1'-0"
MADE BY H.W.W. DATE 7-12-50
CHECKED BY F.J.T. DATE 7/14/50
APPROVED J. J. Dwyer DATE 7-21-50

PROJECT NO. 96-49-03
BRIDGE SHEET NO. 3 OF 9

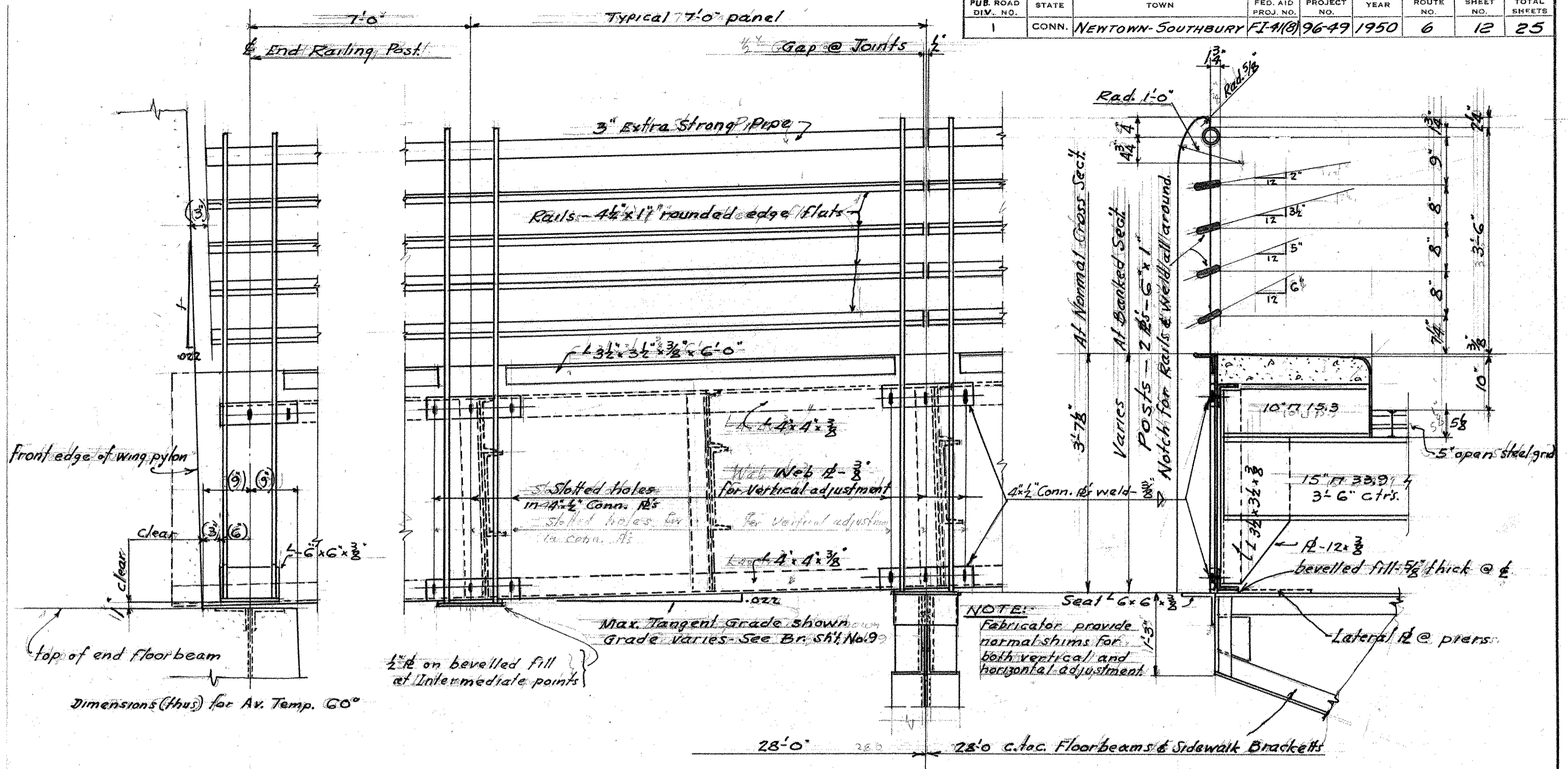


Roadway Expansion Joint plates to be flame cut from a single 24" x 1 3/4" plate
 Teeth - 1/2" Pitch - 3/4" Rounded corners. Horizontal Clearance 1/4" (min. 3/16)
 Conform to cross slopes & grade. North end normal South end banked.
 Dimensions shown in sections are for Av. Temp of 60°



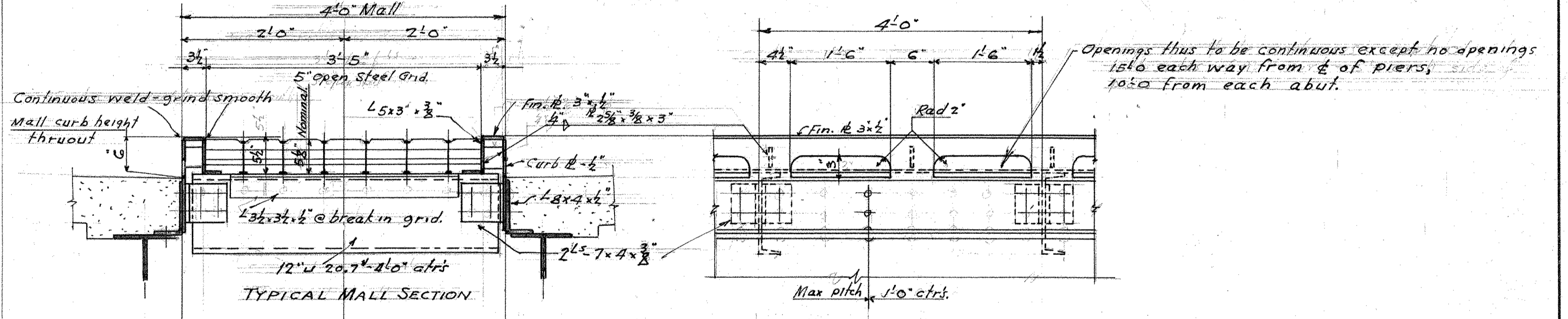
EXPANSION JOINT DETAILS

Scale 3/8" = 1'-0"

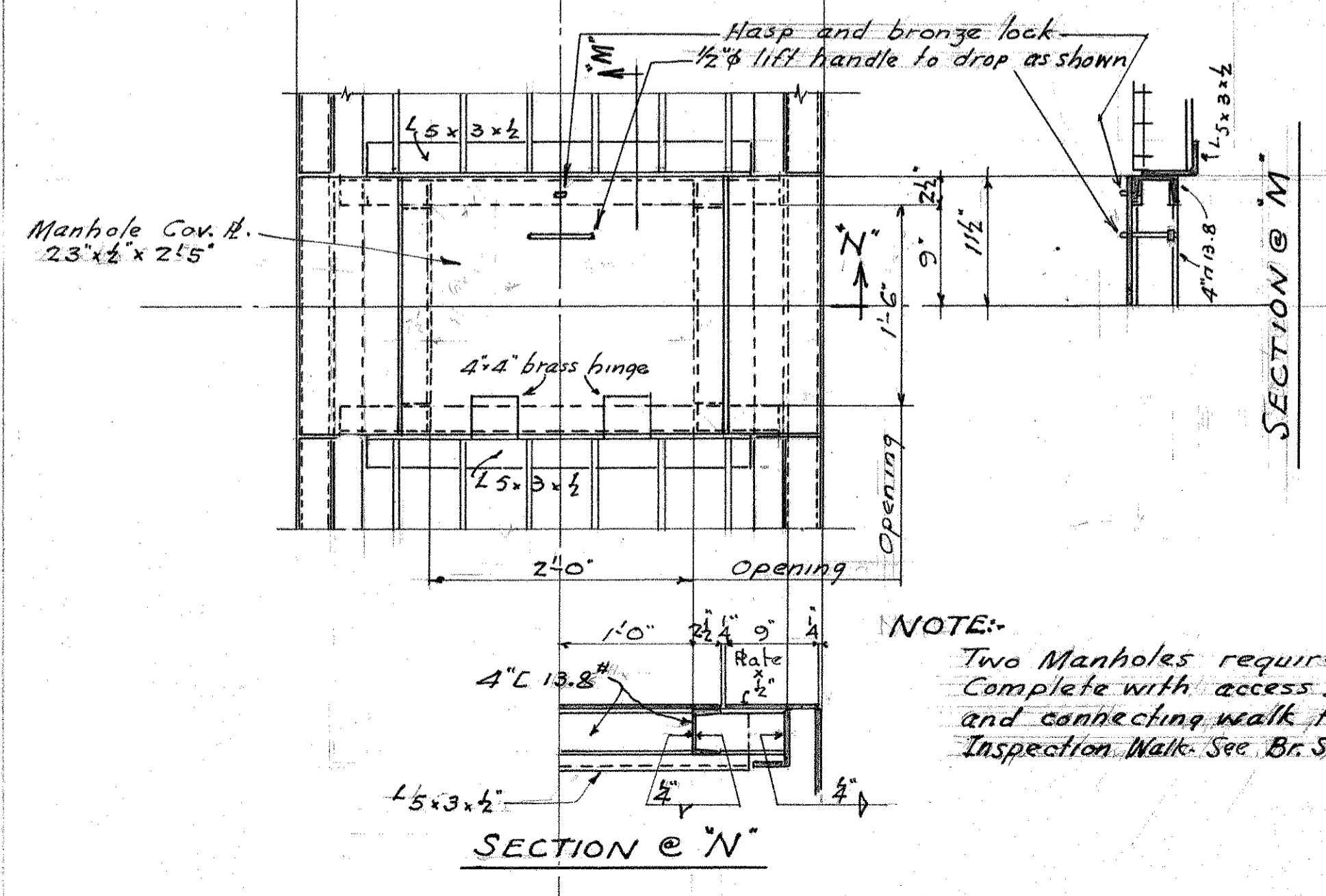


TYPICAL RAILING DETAILS

Scale 3/8" = 1'-0"



TYPICAL MALL SECTION



DETAILS-MALL MANHOLE

Scale 1/2" = 1'-0"

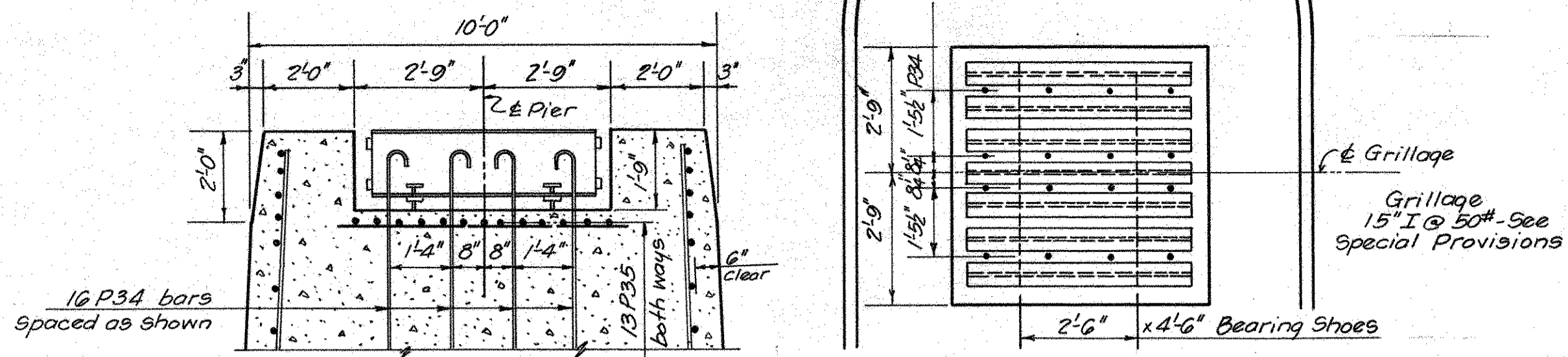
NOTE:
 Two Manholes required
 Complete with access ladder
 and connecting walk to
 Inspection Walk. See Br. Sht. No. 3.

REVISIONS		
NO.	DATE	DESCRIPTION

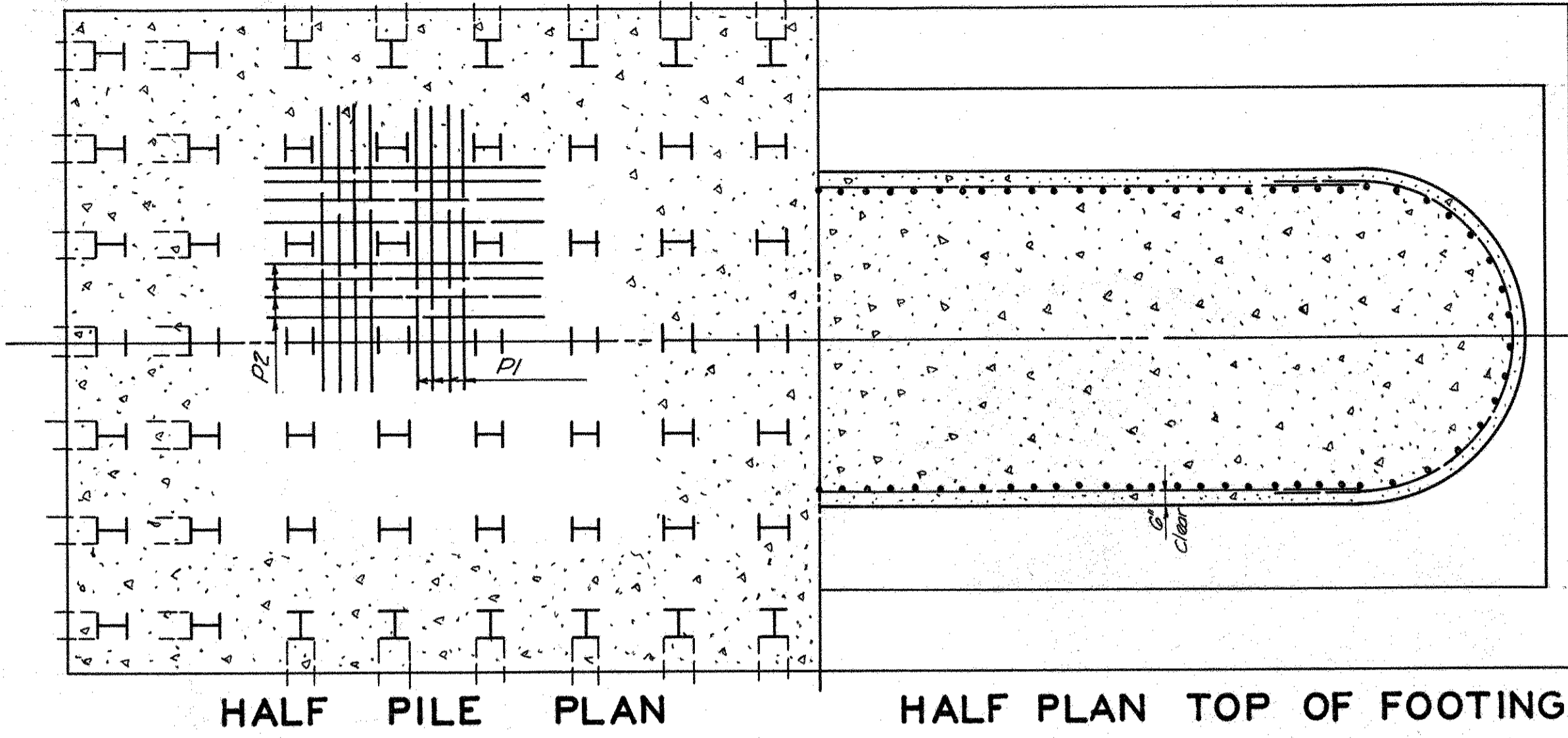
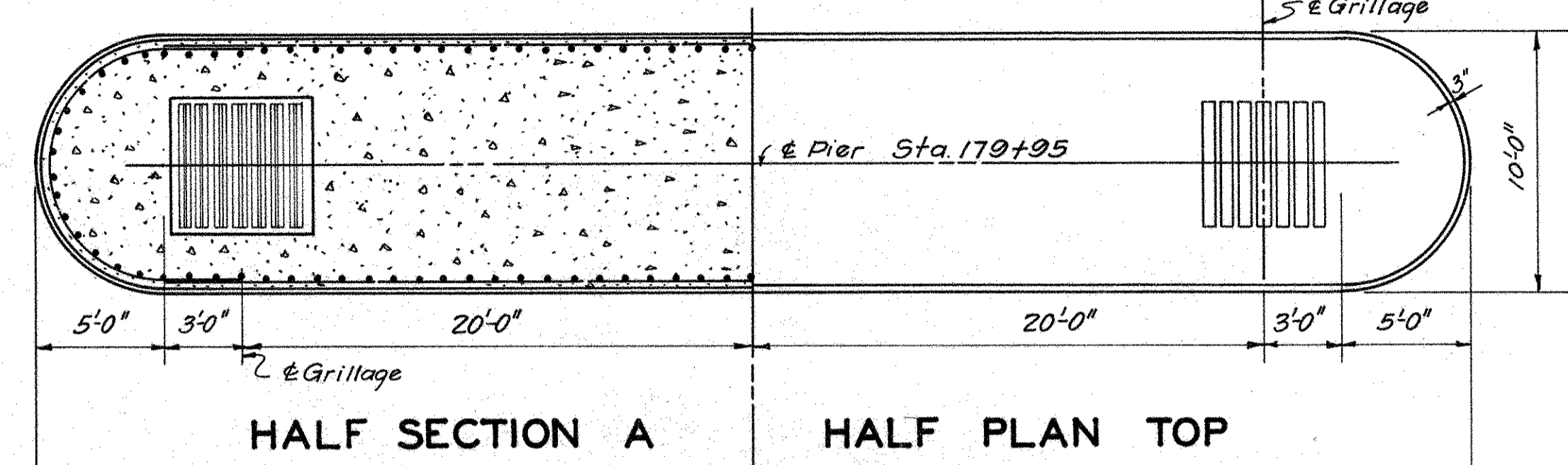
Fed. Aid Proj. FI-41(8)

CONNECTICUT
 STATE HIGHWAY DEPARTMENT
 HOUSATONIC RIVER BRIDGE
 RELOCATION U.S. ROUTE NO. 6
 BETWEEN
 NEWTOWN AND SOUTHBURY
 EXPANSION-MANHOLE
 & RAILING DETAILS

DESIGNED BY F.J.T.
 SCALES - Noted
 MADE BY H.W.W. DATE 7/13/50 PROJECT NO. 96-49-03
 CHECKED BY F.J.T. DATE 7/24/50 BRIDGE SHEET NO. 4 OF 9
 APPROVED J.D. Durey DATE 2/24/51



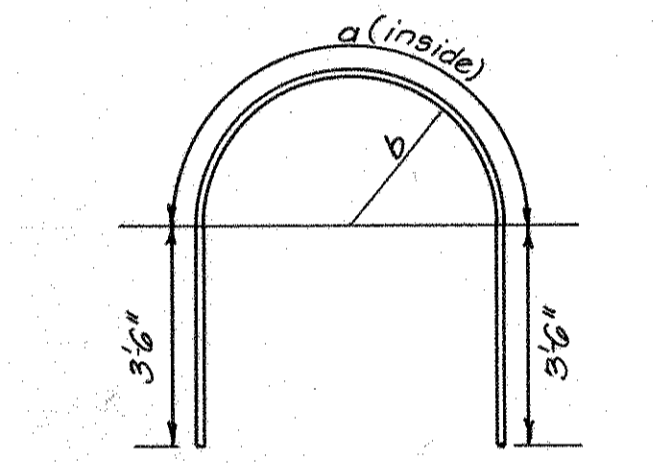
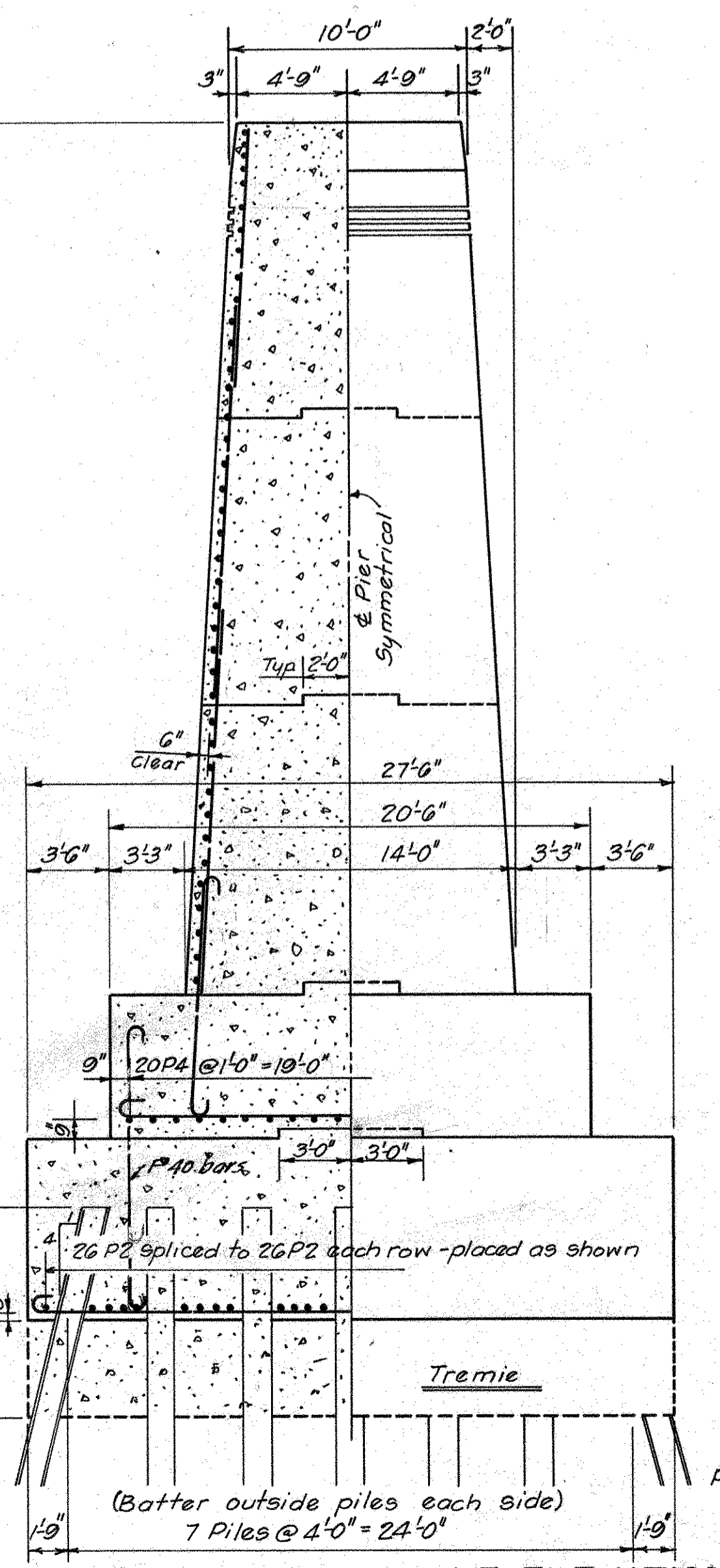
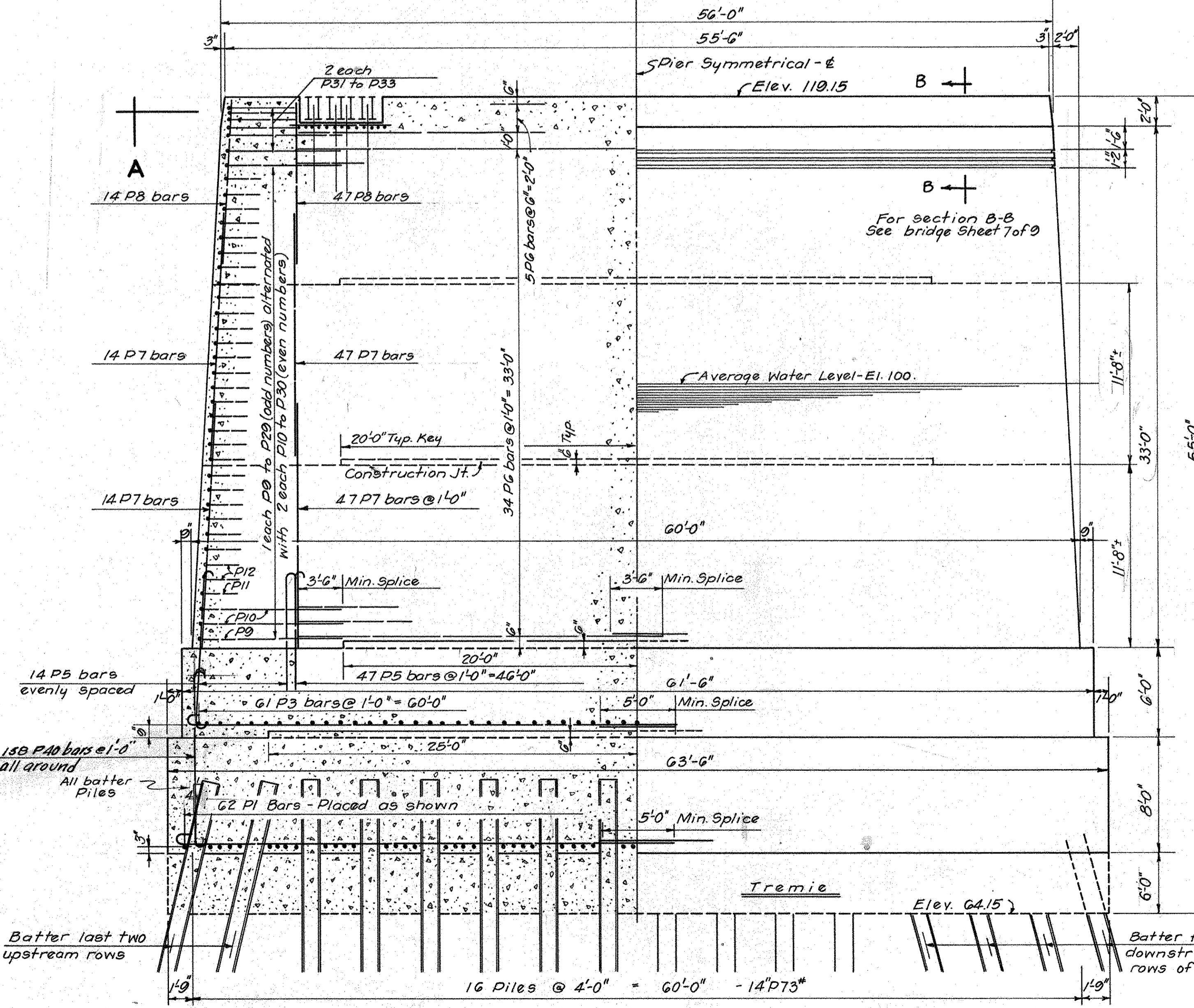
DETAILS AT GRILLAGE



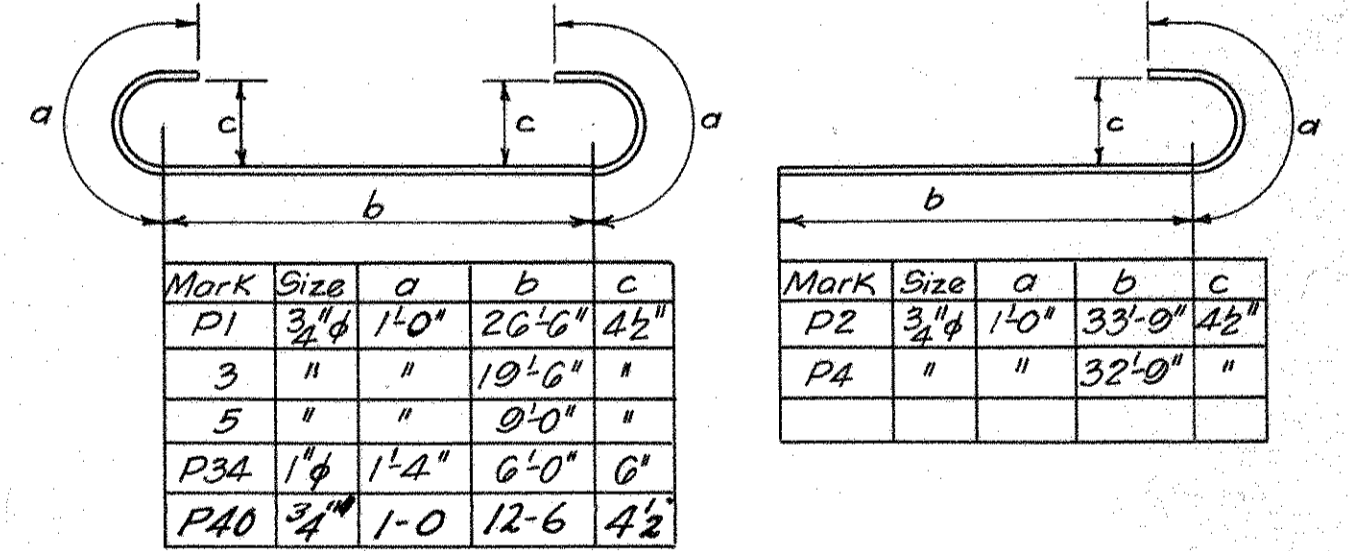
BAR BILL

Mark	No.	Size	Remarks	Length	Weight
P1	62	3/4"	Detailed	28'-6"	2654
2	52			34'-0"	2714
3	61			21'-6"	1970
4	40			33'-9"	2028
5	122		Detailed	11'-0"	2016
6	156		Straight	24'-9"	5799
7	244			16'-6"	6047
8	122		Straight	10'-9"	1970
9	2		Detailed	27'-2"	82
10	4			26'-11"	162
11	2			26'-8"	80
12	4			26'-4"	158
13	2			26'-1"	78
14	4			25'-10"	155
15	2			25'-7"	77
16	4			25'-4"	152
17	2			25'-1"	75
18	4			24'-10"	149
19	2			24'-6"	74
20	4			24'-3"	146
21	2			24'-0"	72
22	4			23'-9"	143
23	2			23'-6"	71
24	4			23'-3"	140
25	2			23'-0"	69
26	4			22'-9"	137
27	2			22'-2"	67
28	4			21'-11"	132
29	2			21'-8"	65
30	4			21'-5"	129
31	4			21'-2"	127
32	4			20'-11"	126
33	4	3/4"		20'-7"	124
34	32	1"	Detailed	8'-8"	741
P35	52	3/4"	Straight	6'-0"	469
PA0	158	3/4"	Detailed	14'-6"	3441
Total - Pier No.1					32639

Note: Prefix all bars on this sheet with No.1



Mark	Size	a	b	Length
P9	3/4"	20'-2"	6'-5"	27'-2"
10		19'-11"	6'-4"	26'-11"
11		19'-8"	6'-3"	26'-8"
12		19'-4"	6'-2"	26'-4"
13		19'-1"	6'-1"	26'-1"
14		18'-10"	6'-0"	25'-10"
15		18'-7"	5'-11"	25'-7"
16		18'-4"	5'-10"	25'-4"
17		18'-1"	5'-9"	25'-1"
18		17'-10"	5'-8"	24'-10"
19		17'-6"	5'-7"	24'-6"
20		17'-3"	5'-6"	24'-3"
21		17'-0"	5'-5"	24'-0"
22		16'-9"	5'-4"	23'-9"
23		16'-6"	5'-3"	23'-6"
24		16'-3"	5'-2"	23'-3"
25		16'-0"	5'-1"	23'-0"
26		15'-9"	5'-0"	22'-9"
27		15'-2"	4'-10"	22'-2"
28		14'-11"	4'-9"	21'-11"
29		14'-8"	4'-8"	21'-8"
30		14'-5"	4'-7"	21'-5"
31		14'-2"	4'-6"	21'-2"
32		13'-11"	4'-5"	20'-11"
P33	3/4"	13'-7"	4'-4"	20'-7"



Note:
Driving Shoes - Use on all piles. Detailed on Br. Sheet 7 of 9
All piles are 14" P73 Bearing Piles and the pile tips are to be driven to Elev. 250 (Rock). Estimated length of piles - 50 feet.
Pile Loads
Average under DL+LL - 39 Tons
Maximum - 56 Tons

REVISIONS

NO.	DATE	DESCRIPTION
1	6-22-51	Pier & Tremie Heights revised

Fed. Aid Proj FI-41(8)

CONNECTICUT STATE HIGHWAY DEPARTMENT
HOUSATONIC RIVER BRIDGE
RELOCATION U.S. ROUTE NO. 6
BETWEEN NEWTOWN AND SOUTHBURY
PIER NO. 1

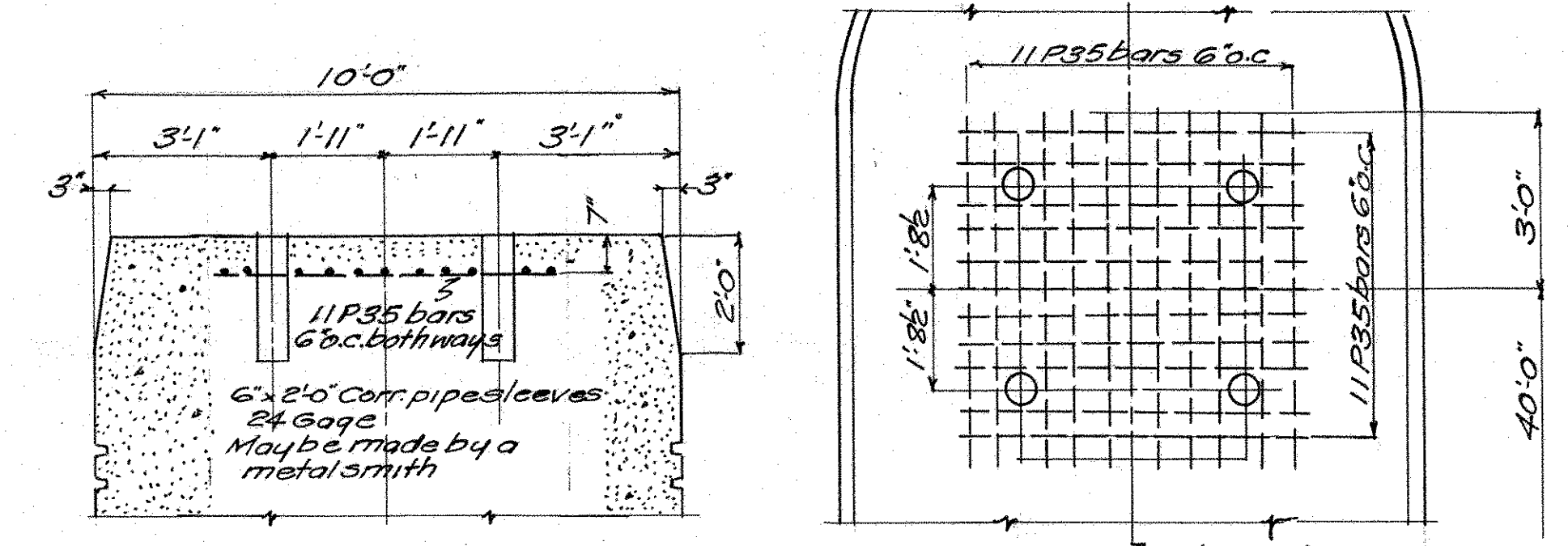
DESIGNED BY: F.J.T.
MADE BY: G.A.H. Tr. C.L.L. DATE: 9/21/50
CHECKED BY: F.J.T. DATE: 7/24/50
APPROVED: J.D. Dwyer DATE: 12/16/50

PROJECT NO. 96-49-03
BRIDGE SHEET NO. 5 of 9
STANDARD NO. 17

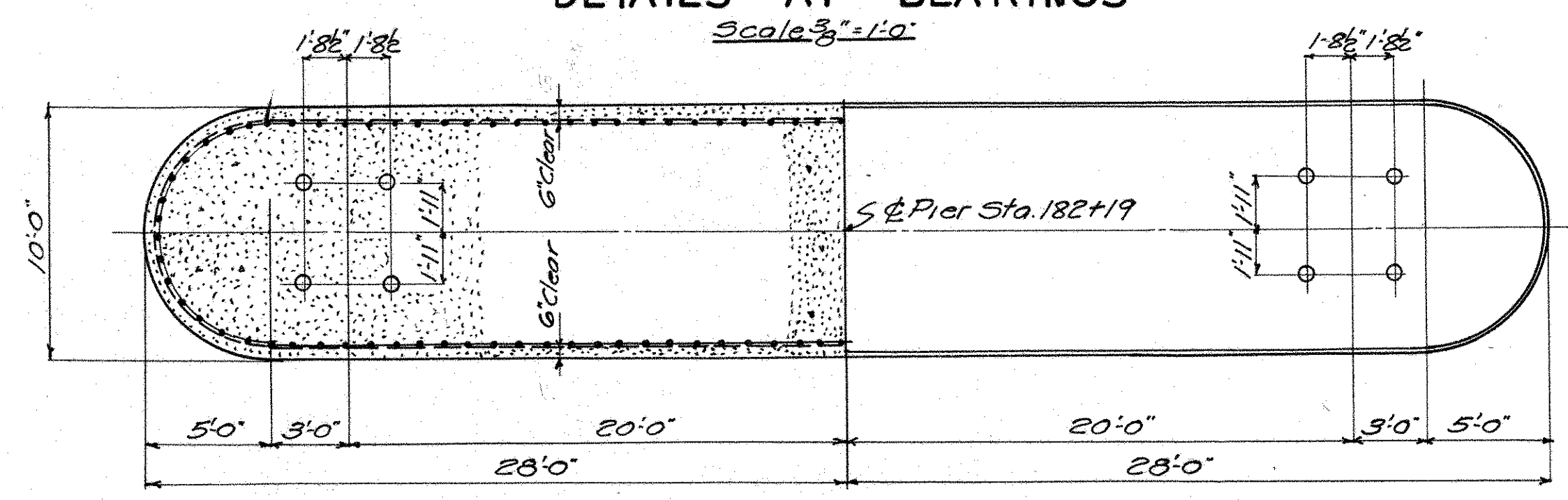
BAR BILL

Mark	No	Size	Remarks	Length	Weight
P1	72	3/4"	Detailed	28'6"	3082
P3	65	"	"	21'6"	2099
P5	122	"	"	11'0"	2016
P6	148	"	Straight	24'9"	5502
P7	244	"	"	16'6"	6047
P9	2	"	Detailed	27'2"	82
P10	4	"	"	26'10"	161
P11	2	"	"	26'8"	80
P12	4	"	"	26'5"	159
P13	2	"	"	26'1"	73
P14	4	"	"	25'10"	155
P15	2	"	"	25'4"	76
P16	4	"	"	25'1"	151
P17	2	"	"	24'10"	75
P18	4	"	"	24'7"	148
P19	2	"	"	24'3"	73
P20	4	"	"	24'0"	144
P21	2	"	"	23'9"	71
P22	4	"	"	23'6"	141
P23	2	"	"	23'3"	70
P24	4	"	"	23'0"	138
P25	2	"	"	22'9"	68
P26	4	"	"	22'5"	135
P27	2	"	"	22'2"	67
P28	4	"	"	21'11"	132
P29	2	"	"	21'5"	64
P30	4	"	"	21'2"	127
P31	2	"	"	20'11"	63
P32	4	"	"	20'10"	125
P33	2	3/4"	Detailed	20'8"	62
P35	44	3/4"	Straight	6'0"	397
P36	52	"	Detailed	38'6"	3007
P37	40	"	"	36'0"	2163
P38	122	3/4"	Straight	9'6"	1558
P40	166	"	Detailed	14'6"	3615

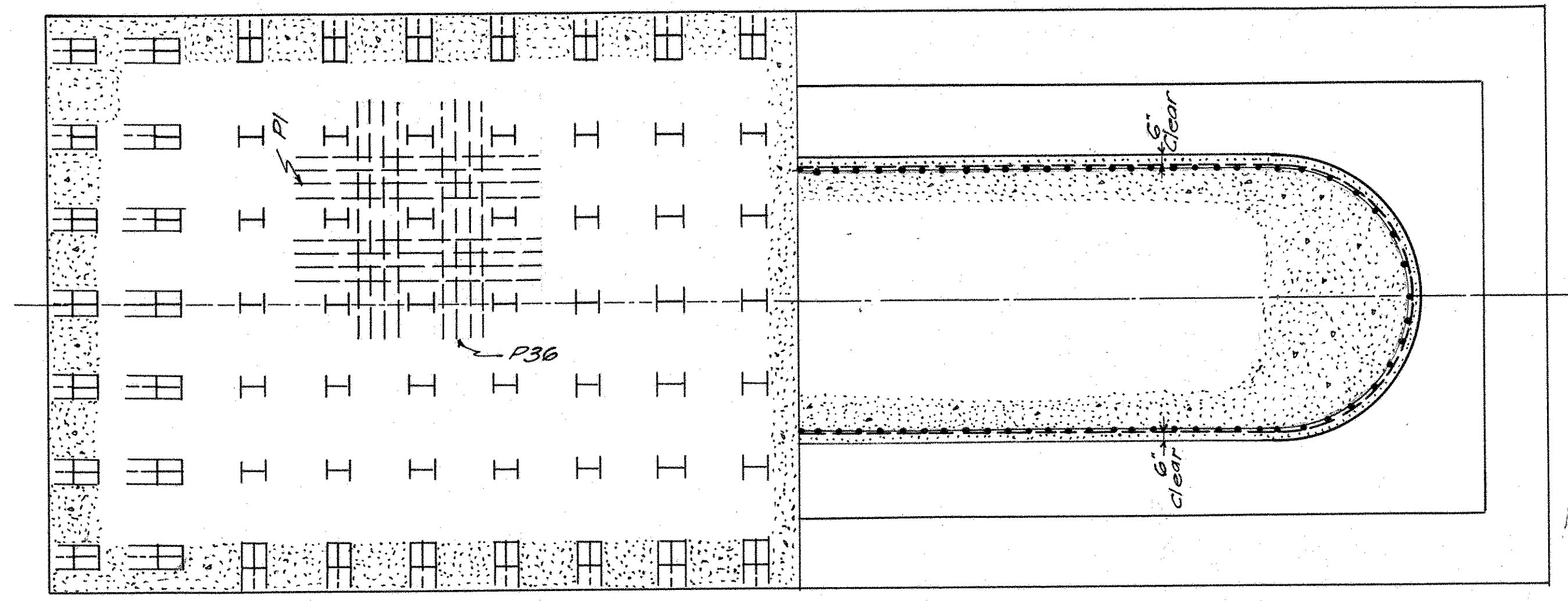
Total Weight 32131
 Prefix all bars with No. 2



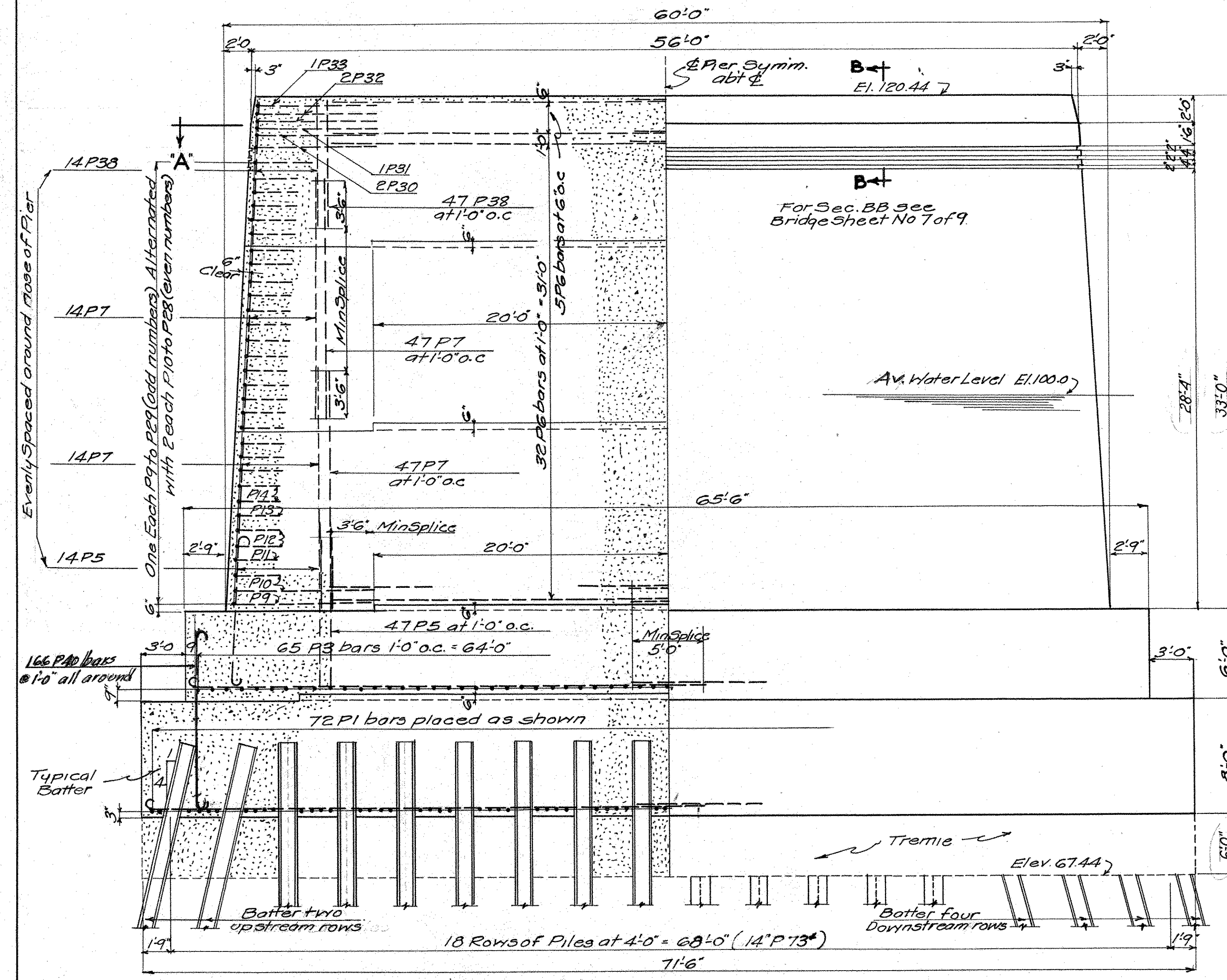
DETAILS AT BEARINGS
 Scale 3/8"=1'-0"



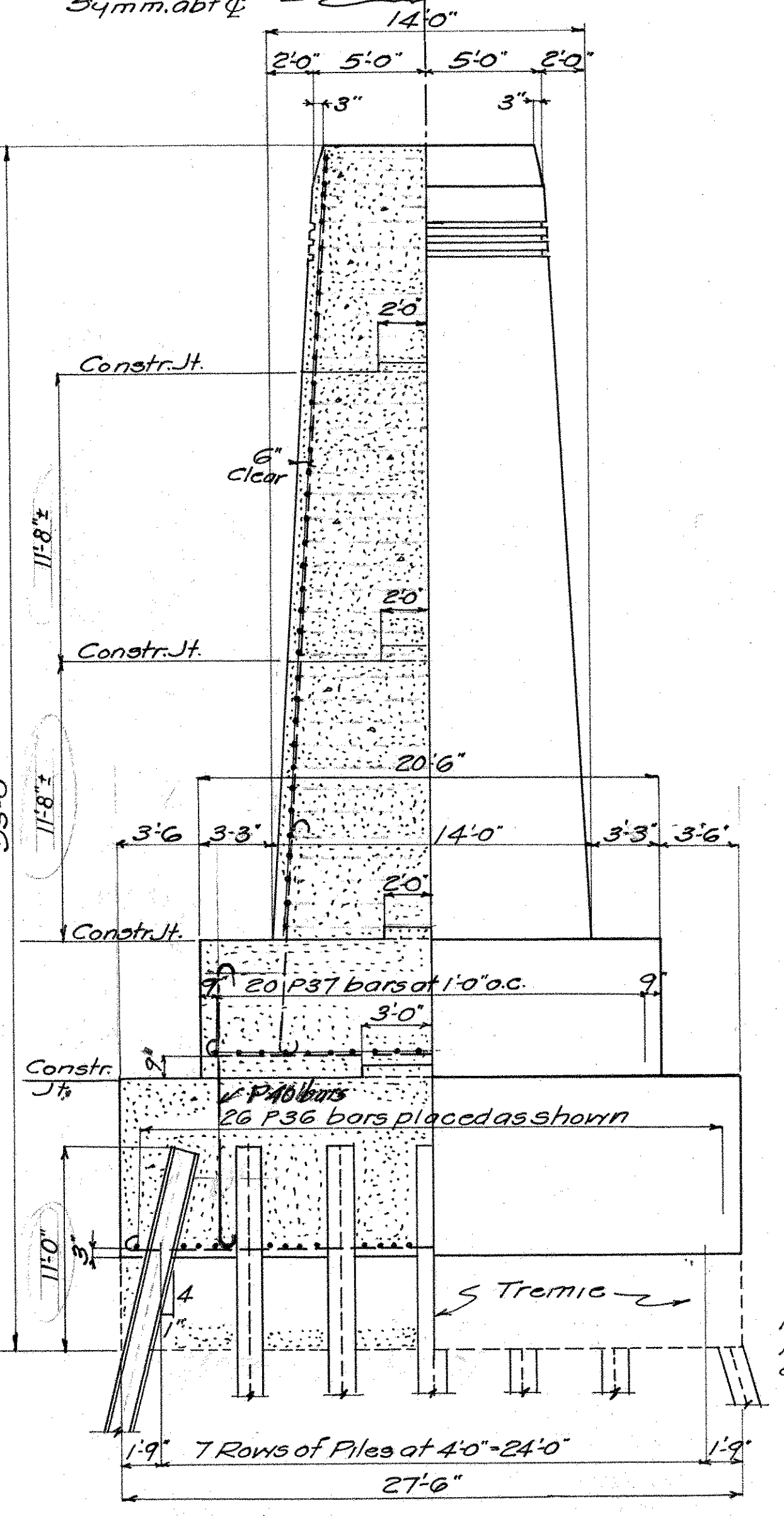
HALF SECTION "A" HALF PLAN TOP
 Scale 3/8"=1'-0"



HALF PILE PLAN HALF PLAN TOP OF FOOTING
 Scale 3/8"=1'-0"



HALF SECTION HALF ELEVATION
 Scale 3/8"=1'-0"



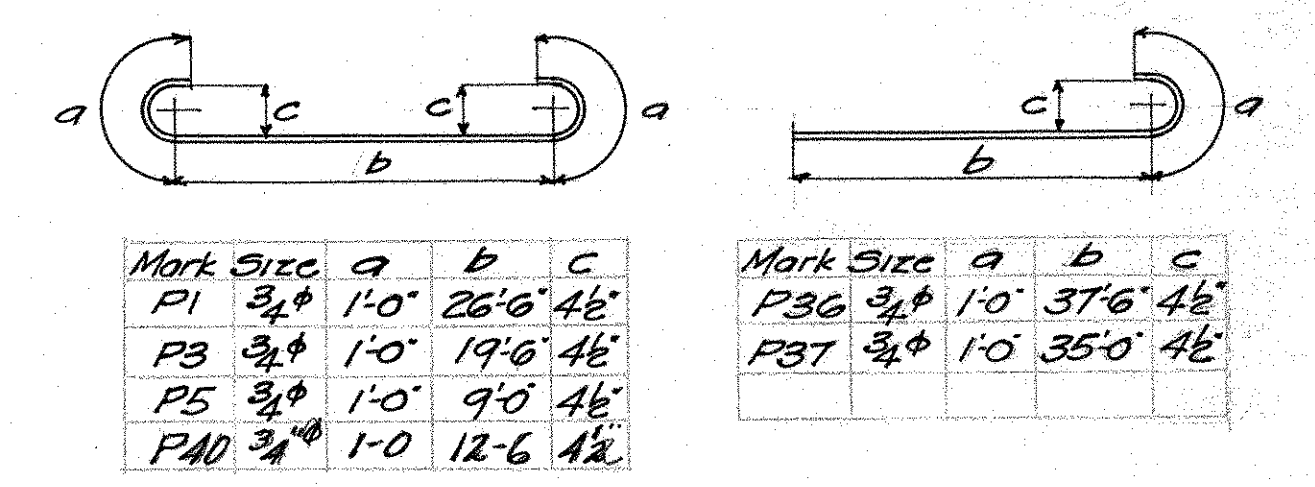
HALF SECTION HALF ELEVATION
 Scale 3/8"=1'-0"

Mark	Size	a	b	Length
P9	3/4"	20'2"	6'5"	27'2"
P10	"	19'10"	6'4"	26'10"
P11	"	19'8"	6'3"	26'8"
P12	"	19'5"	6'2"	26'5"
P13	"	19'1"	6'1"	26'1"
P14	"	18'10"	6'0"	25'10"
P15	"	18'4"	5'10"	25'4"
P16	"	18'1"	5'9"	25'1"
P17	"	17'10"	5'8"	24'10"
P18	"	17'7"	5'7"	24'7"
P19	"	17'3"	5'6"	24'3"
P20	"	17'0"	5'5"	24'0"
P21	"	16'9"	5'4"	23'9"
P22	"	16'6"	5'3"	23'6"
P23	"	16'3"	5'2"	23'3"
P24	"	16'0"	5'1"	23'0"
P25	"	15'9"	5'0"	22'9"
P26	"	15'5"	4'11"	22'5"
P27	"	15'2"	4'10"	22'2"
P28	"	14'11"	4'9"	21'11"
P29	"	14'5"	4'7"	21'5"
P30	"	14'2"	4'6"	21'2"
P31	"	13'11"	4'5"	20'11"
P32	"	13'10"	4'5"	20'10"
P33	3/4"	13'8"	4'4"	20'8"

Driving shoes used on all Piles
 Detailed on Bridge Sheet # 7 of 9
 Note: All piles are 14" P73" bearing piles and the pile tips are to be driven to Elev. 18.0 (rock)
 Est. Length of Piles 60'

Pile Loads
 Av. under O.L.+L.L. 36 Tons
 Maximum 52 Tons

BAR DETAILS



REVISIONS		
NO.	DATE	DESCRIPTION
1	6-22-51	Pier & Tremie Heights revised.

Fed. Aid Proj. FL-4(8)

CONNECTICUT
 STATE HIGHWAY DEPARTMENT
 HOUSATONIC RIVER BRIDGE
 RELOCATION U.S. ROUTE NO. 6
 BETWEEN
 NEWTOWN AND SOUTHBURY
 PIER NO. 2

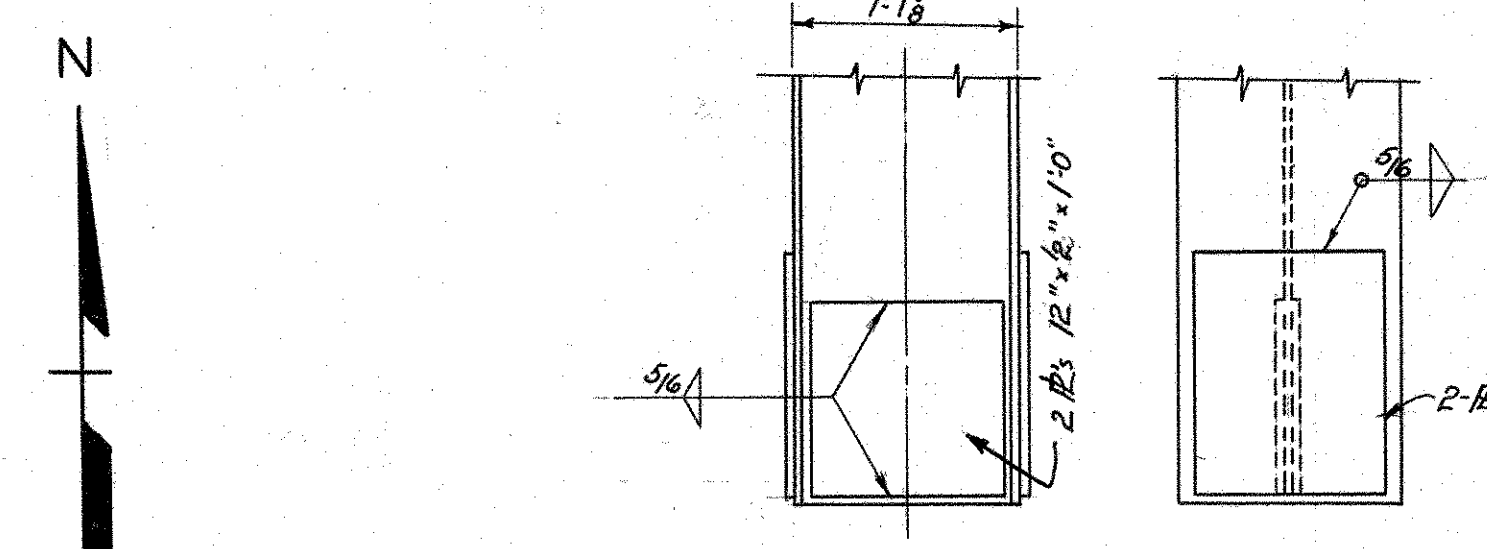
DESIGNED BY F.L.T.
 SCALES 3/16" & 3/8" = 1'-0"
 MADE BY GAH, T.F., H.E.J. DATE 6/21/50 PROJECT NO. 96-47-03
 CHECKED BY F.L.T. DATE 7/21/50 BRIDGE SHEET NO. 6 OF 9
 APPROVED J.D. DUNN DATE 8/14/50

FED. ROAD DIST. NO.	STATE	TOWN	FED. AID PROJ. NO.	PROJECT NO.	YEAR	ROUTE NO.	SHEET NO.	TOTAL SHEETS
1	CONN.	NEWTOWN-SOUTHBURY	FI-41(8)	96-49	1950	6	15	25

BAR BILL

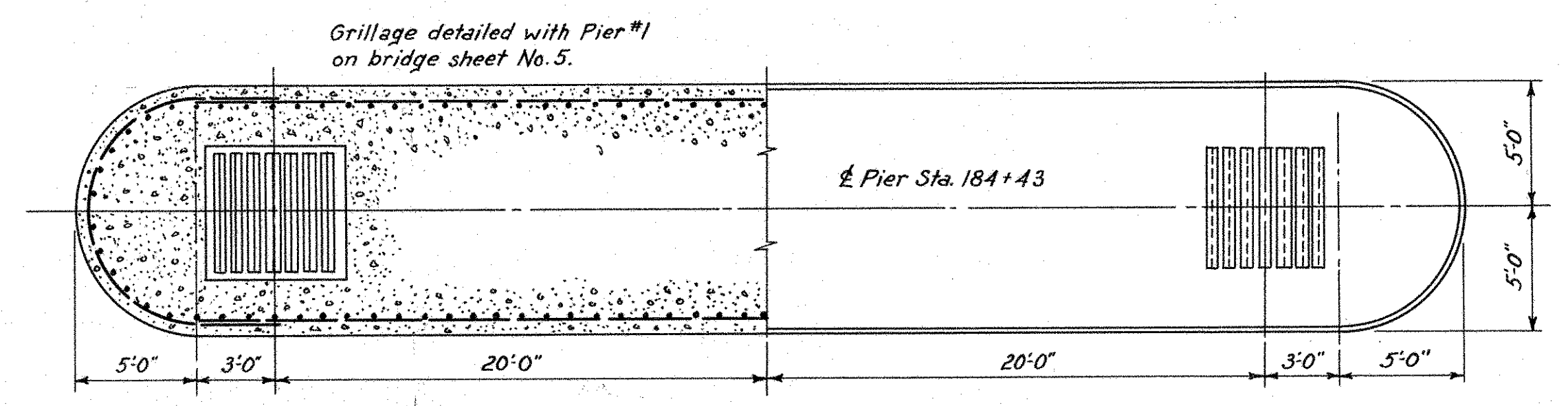
MARK	SIZE	No.	TYPE	A	B	C	LENGTH	WEIGHT
P2	3/4"	44	C	1'-0"	33'-9"	4 1/2"	34'-9"	2297
P4	"	34	C	1'-0"	32'-9"	4 1/2"	33'-9"	1724
P5	"	118	B	1'-0"	9'-0"	4 1/2"	11'-0"	1950
P6	"	104	Str.	---	---	---	24'-9"	3866
P7	"	118	Str.	---	---	---	16'-6"	2924
P8	"	118	Str.	---	---	---	10'-9"	1905
P17	"	4	A	18'-1"	5'-9"	3'-6"	25'-1"	151
P18	"	2	A	17'-10"	5'-8"	3'-6"	24'-10"	75
P19	"	4	A	17'-6"	5'-7"	3'-6"	24'-6"	147
P20	"	2	A	17'-3"	5'-6"	3'-6"	24'-3"	73
P21	"	4	A	17'-0"	5'-5"	3'-6"	24'-0"	144
P22	"	2	A	16'-9"	5'-4"	3'-6"	23'-9"	71
P23	"	4	A	16'-6"	5'-3"	3'-6"	23'-6"	141
P24	"	2	A	16'-3"	5'-2"	3'-6"	23'-3"	70
P25	"	4	A	16'-0"	5'-1"	3'-6"	23'-0"	138
P26	"	2	A	15'-8"	5'-0"	3'-6"	22'-8"	68
P27	"	4	A	15'-5"	4'-11"	3'-6"	22'-5"	135
P28	"	2	A	15'-2"	4'-10"	3'-6"	22'-2"	67
P29	"	4	A	14'-11"	4'-9"	3'-6"	21'-11"	132
P30	"	2	A	14'-8"	4'-8"	3'-6"	21'-8"	65
P31	"	4	A	14'-5"	4'-7"	3'-6"	21'-5"	129
P32	"	2	A	14'-2"	4'-6"	3'-6"	21'-2"	64
P33	"	4	A	13'-11"	4'-5"	3'-6"	20'-11"	126
P34	1 1/4"	32	B	1'-4"	6'-0"	6"	8'-8"	741
P35	3/4"	52	Str.	---	---	---	6'-0"	469
P41	"	62	B	1'-0"	22'-6"	4 1/2"	24'-6"	2282
P42	"	60	B	1'-0"	17'-0"	4 1/2"	19'-0"	1713
P40	"	154	B	1'-0"	12'-6"	4 1/2"	14'-6"	3334
								Total 25021

NOTE:
Prefix all bars on this sheet with No. 3.

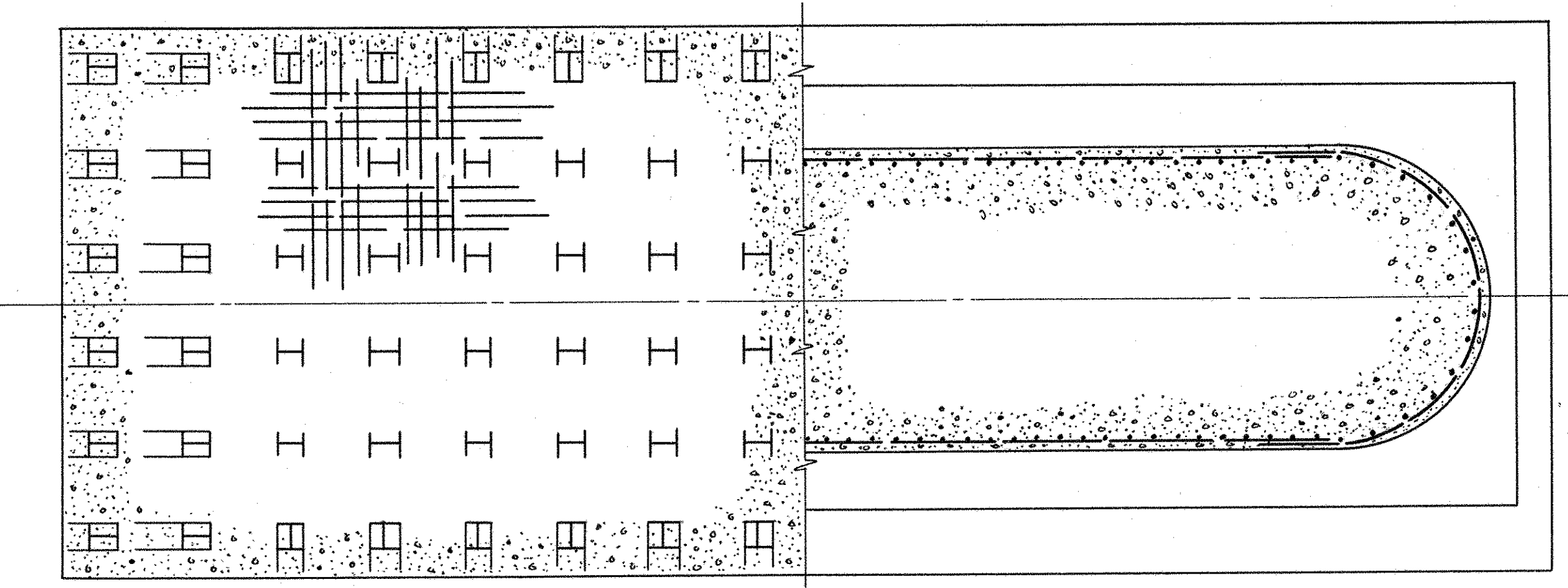


NOTE:
Fabrication cost of Driving Shoe to be included in Bid Price for furnishing steel piles. This shoe to be used on all piles.

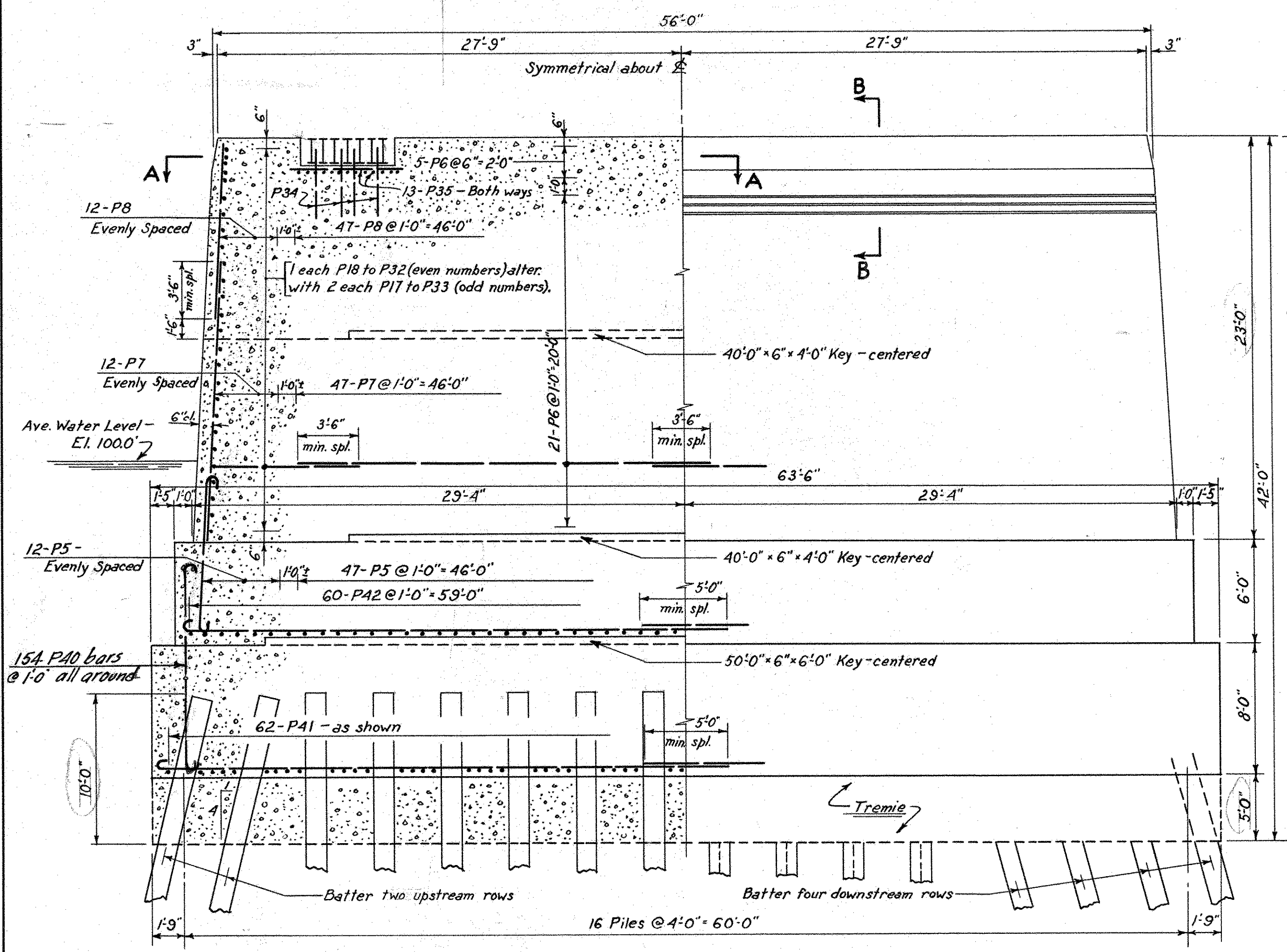
DETAIL OF DRIVING SHOE



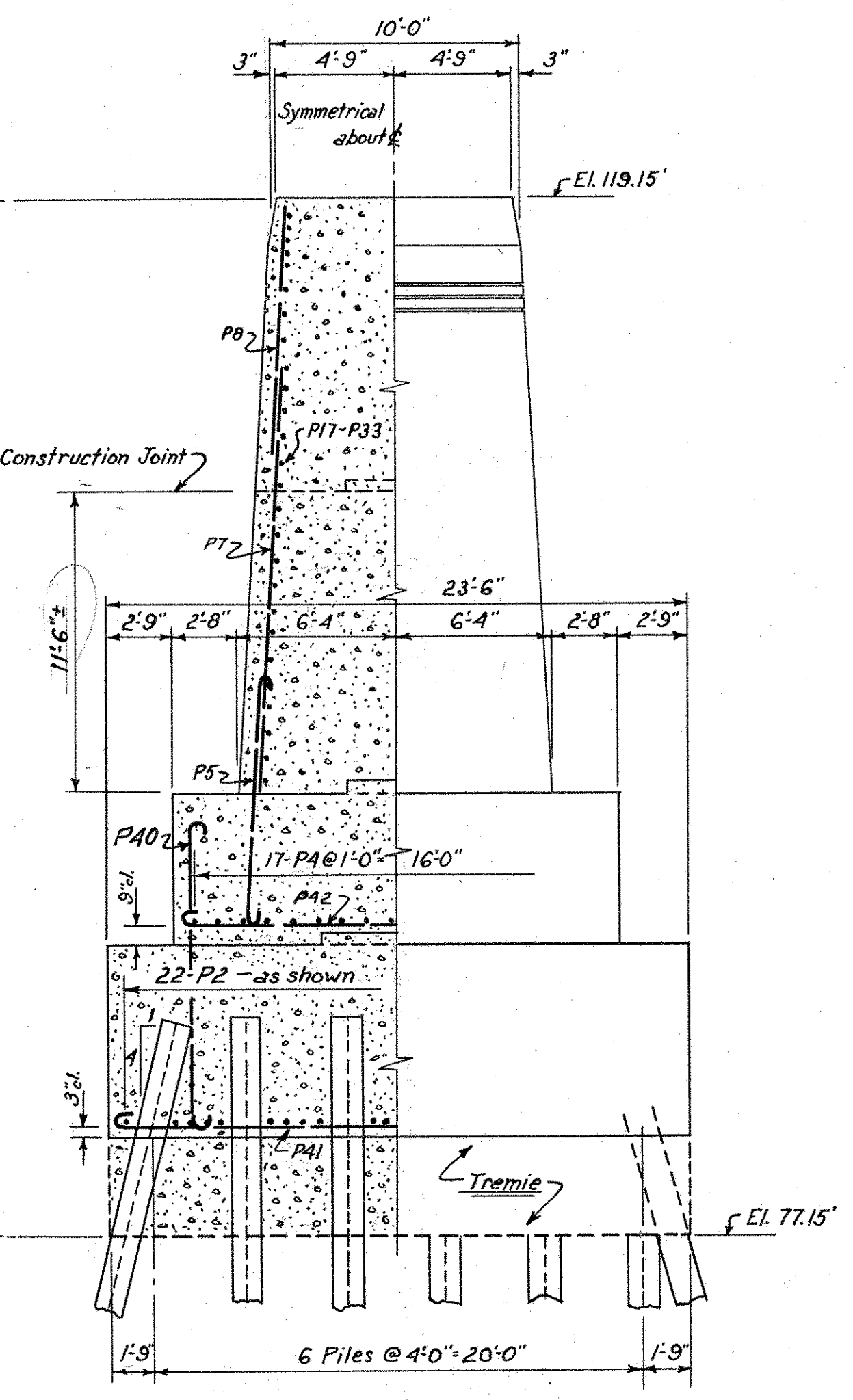
HALF SECTION A-A HALF PLAN TOP



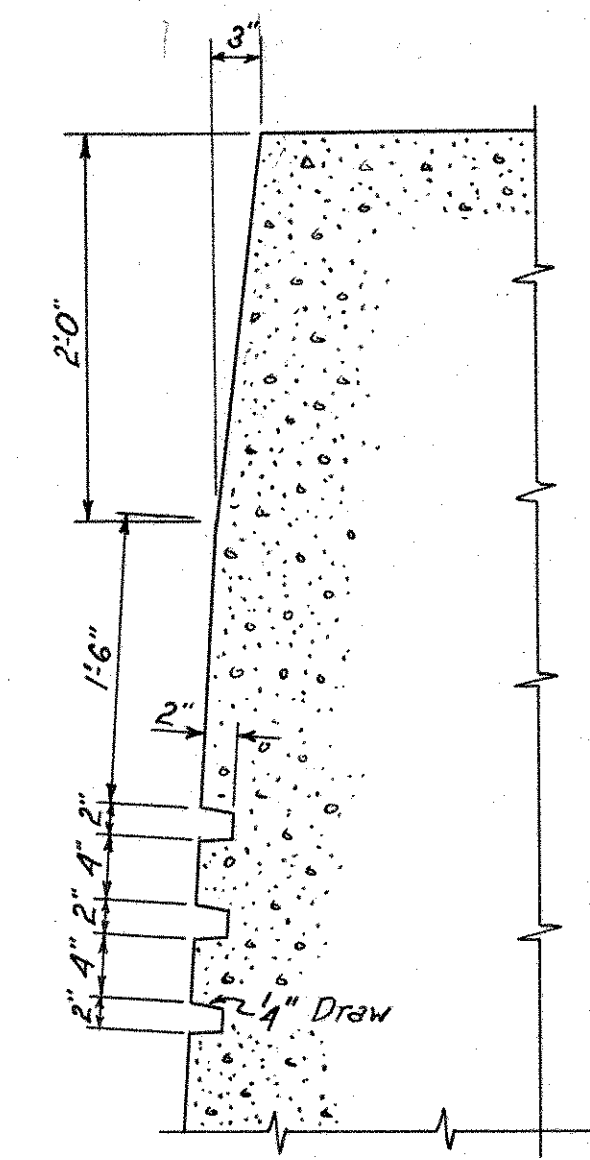
HALF PILE PLAN HALF PLAN TOP OF FOOTING



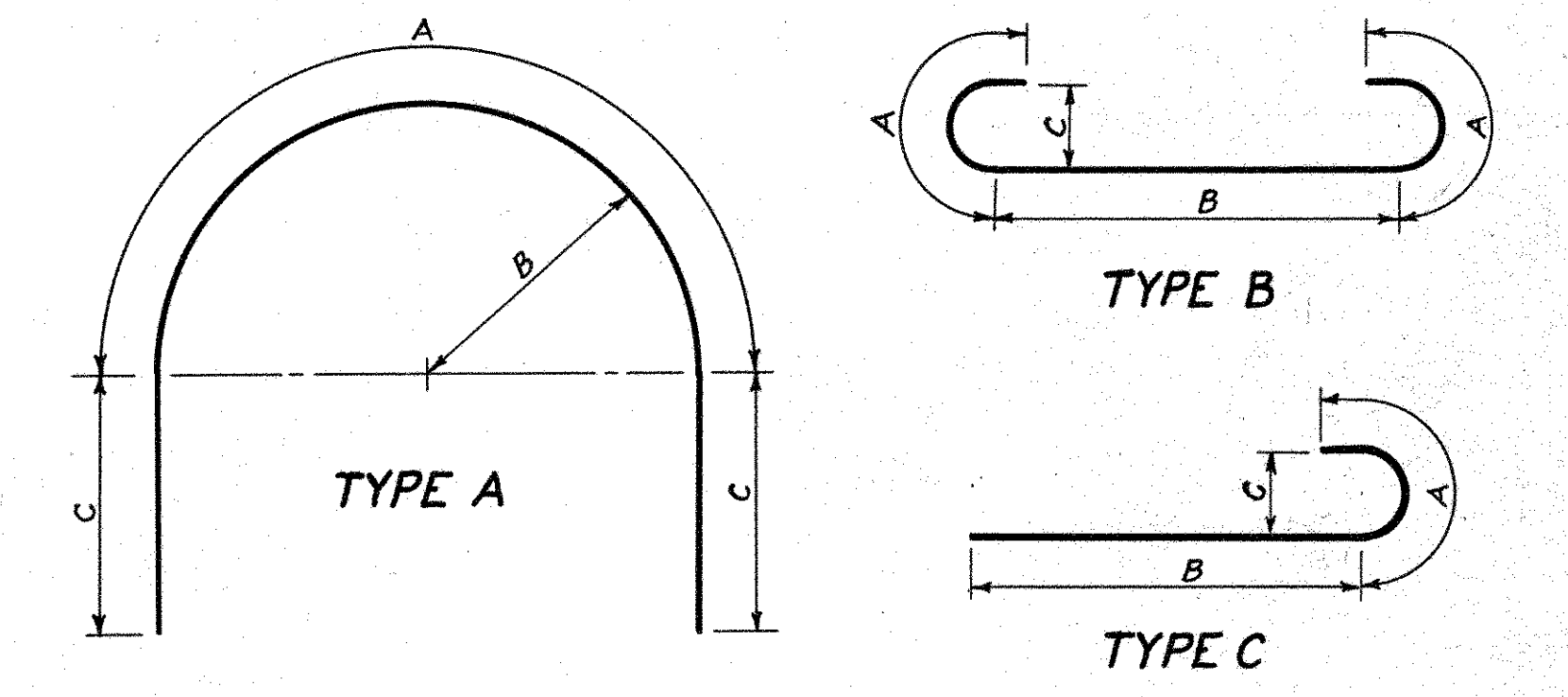
HALF SECTION HALF ELEVATION



HALF SECTION-HALF ELEVATION



SECTION B-B

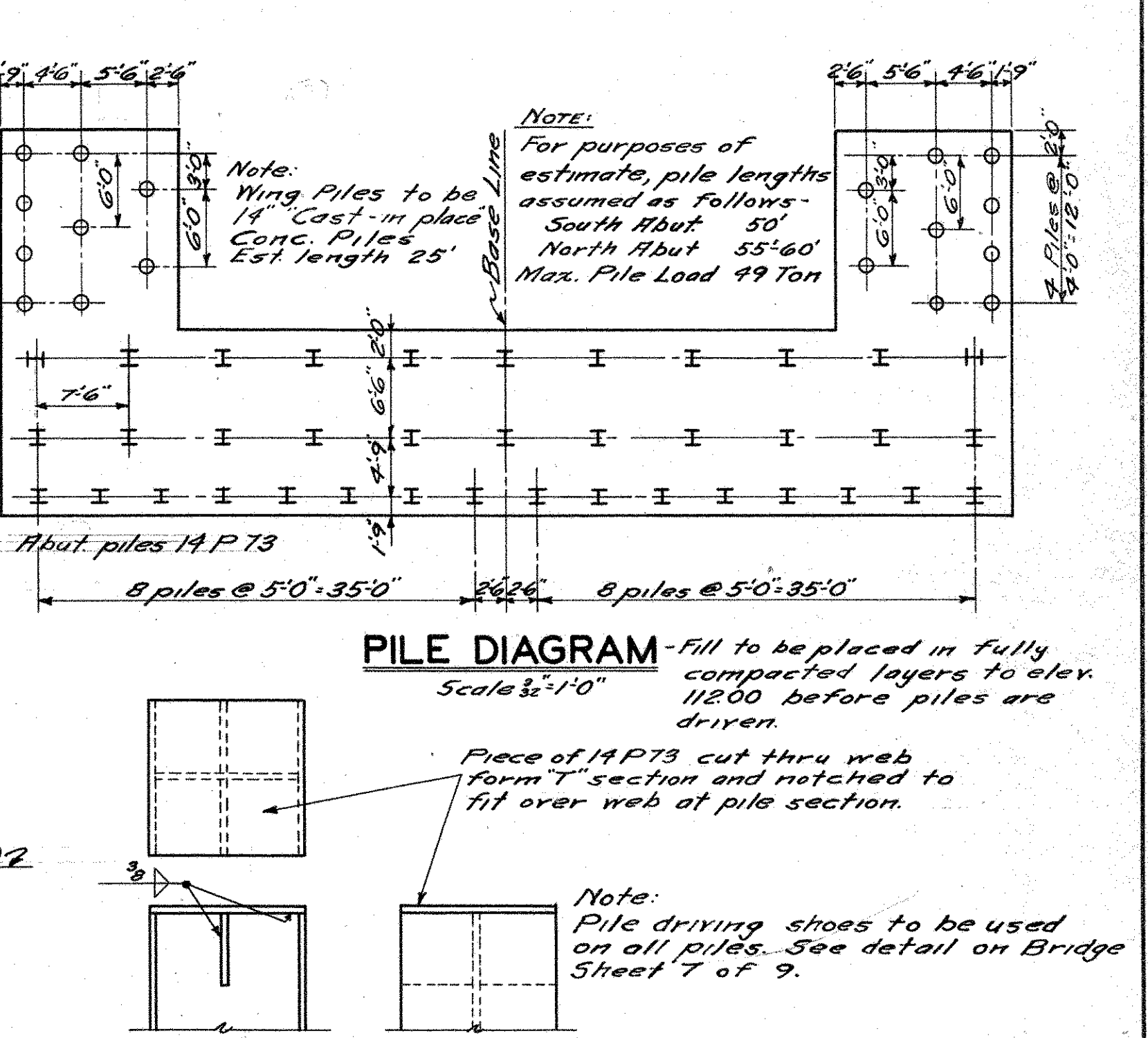
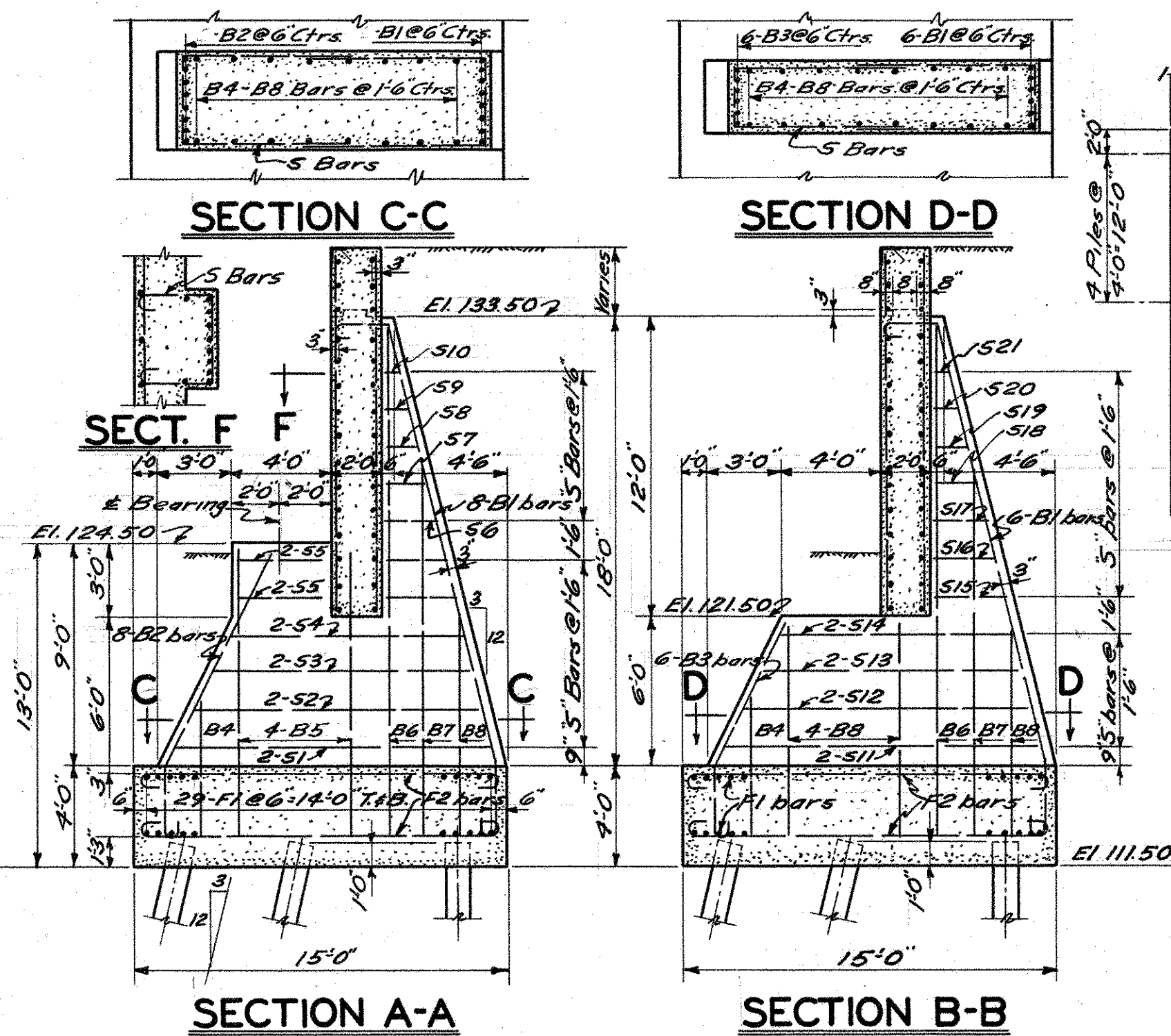
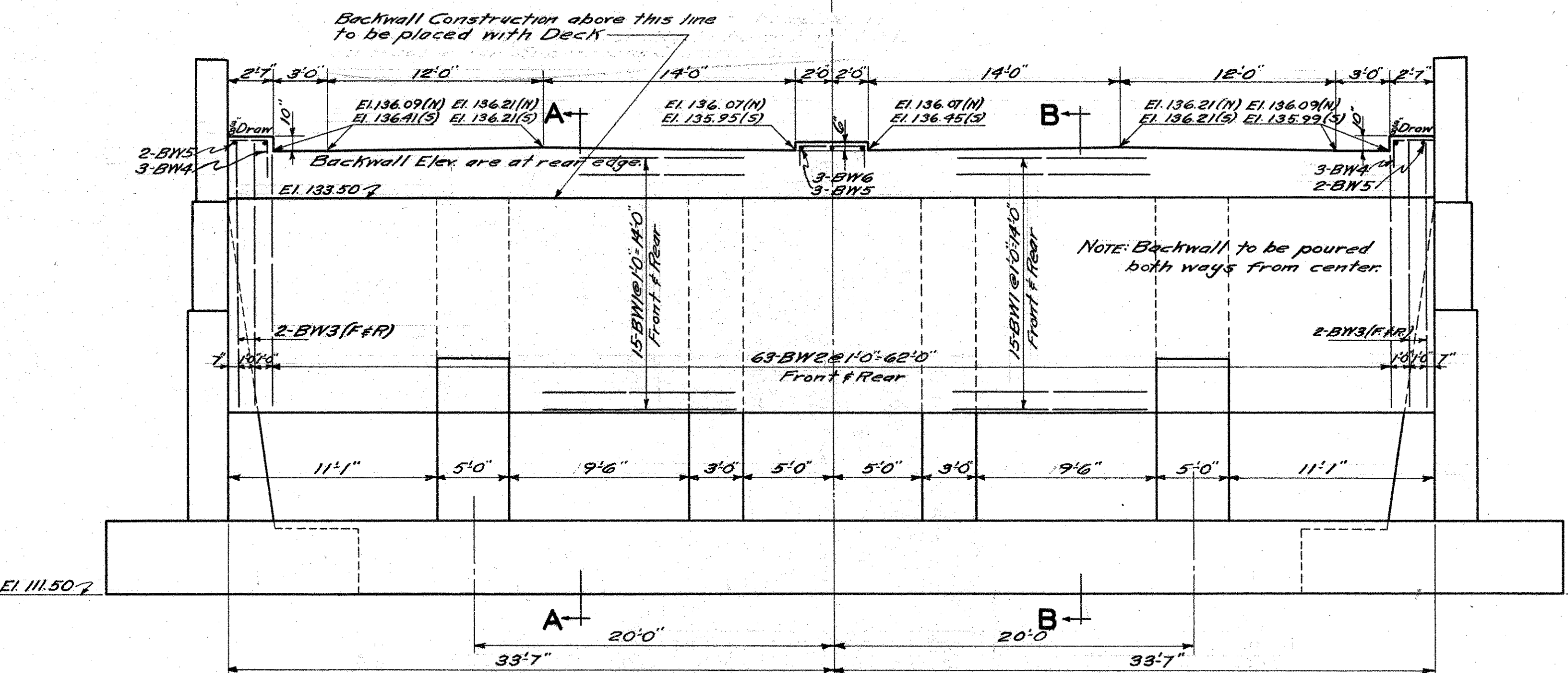
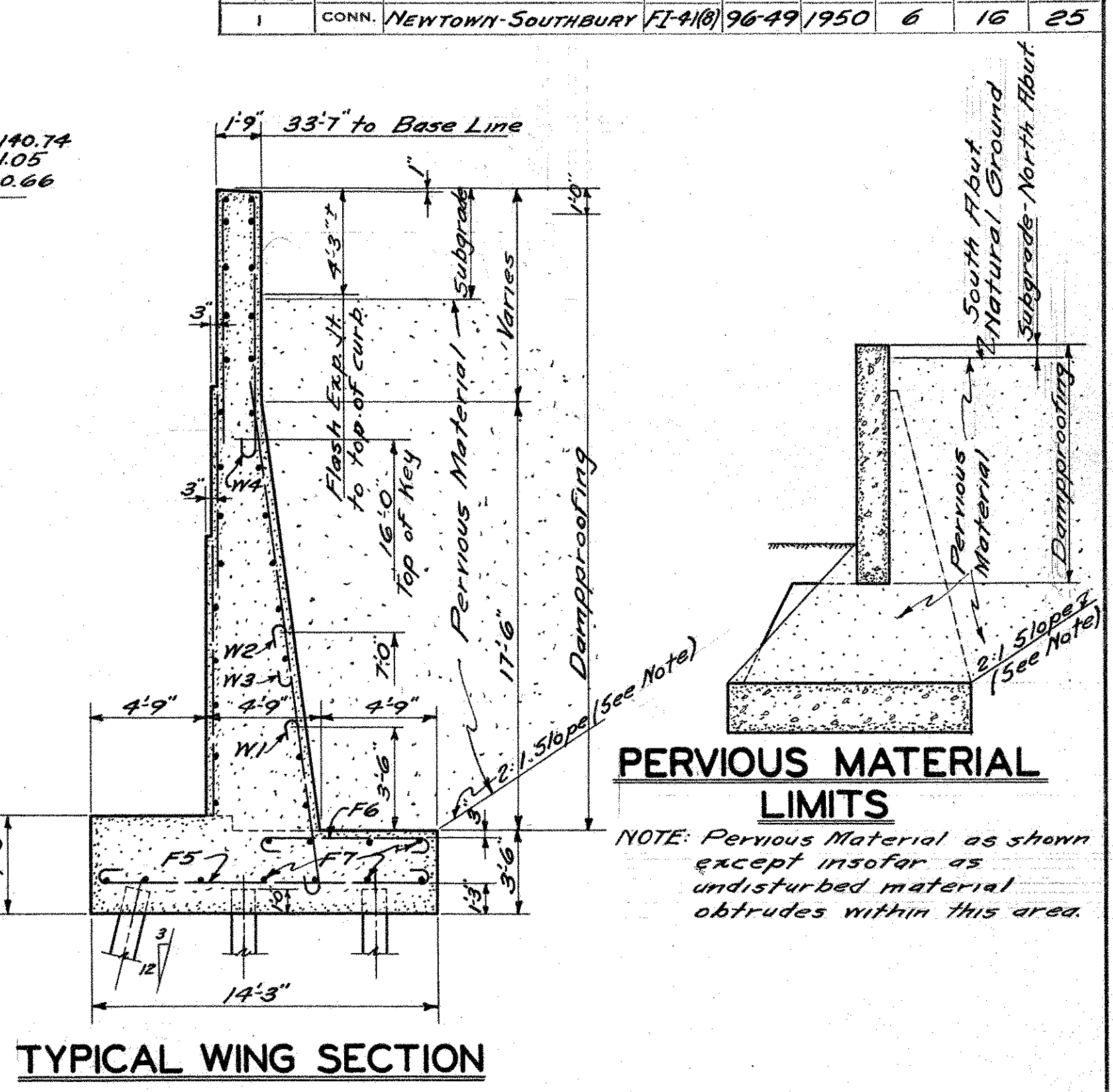
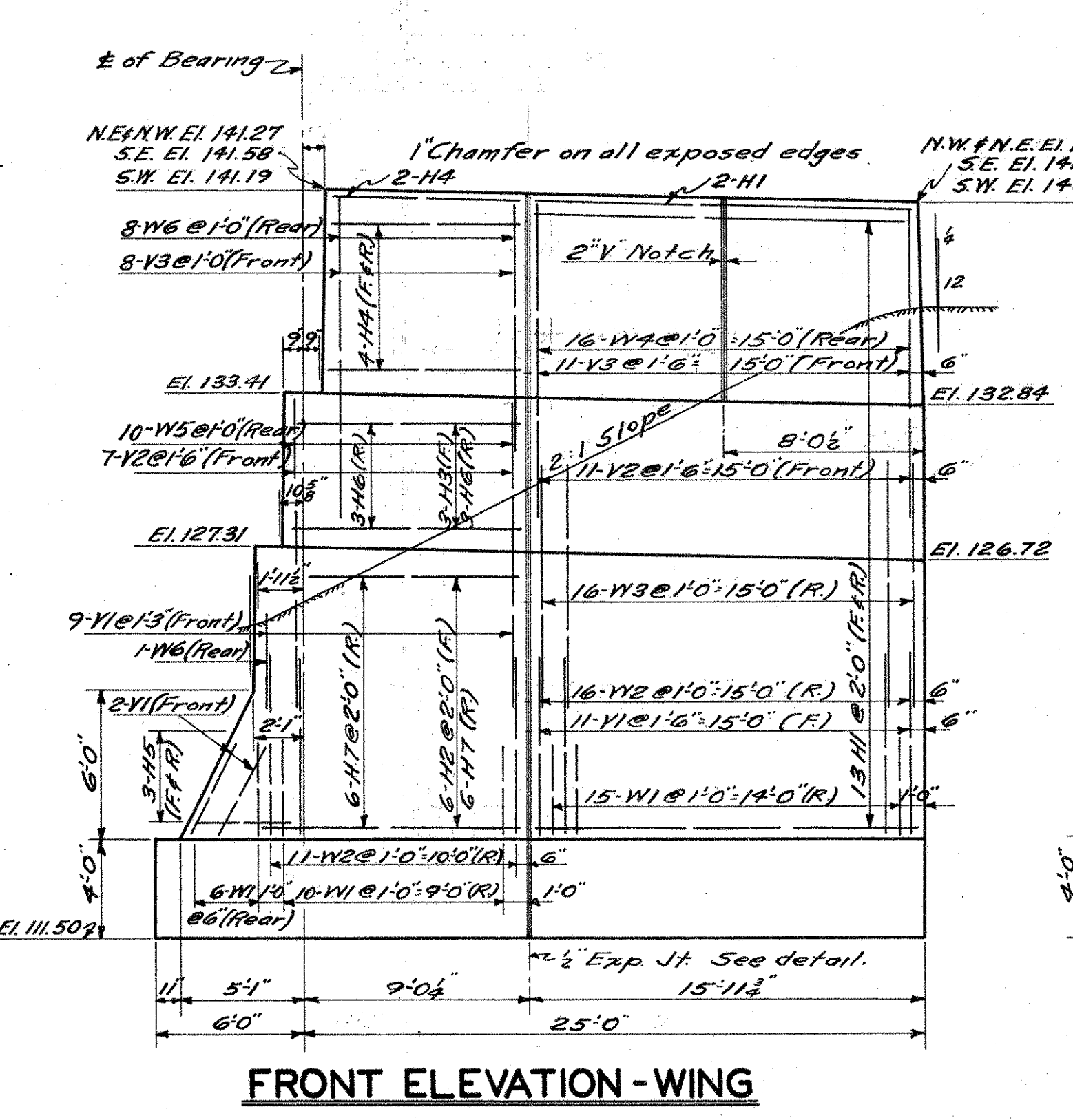
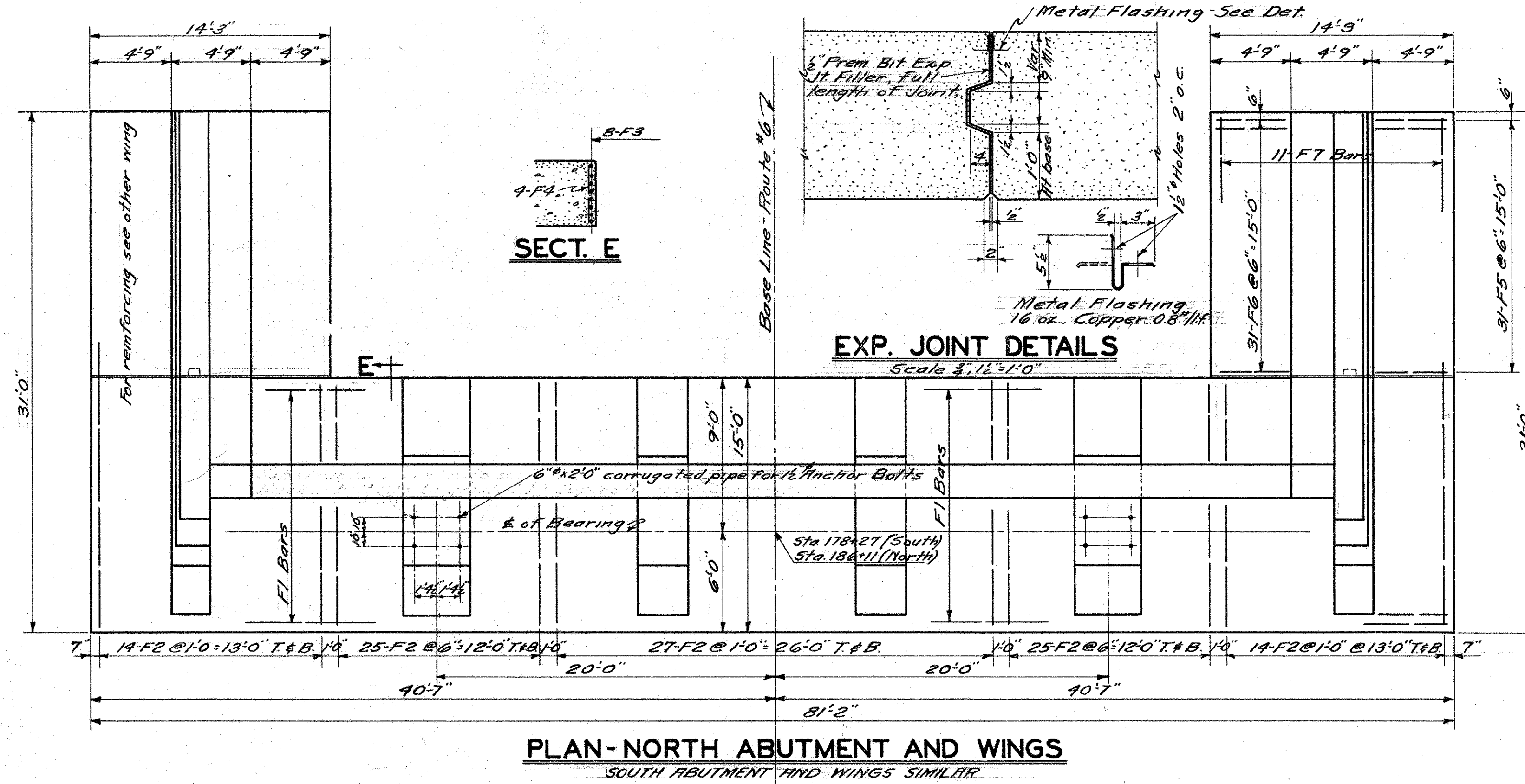


REVISIONS		
NO.	DATE	DESCRIPTION
1	3-13-51	change in Driving Shoe
2	6-22-51	Pier & Tremie Heights revised

FED. AID PROJ. FI-41(8)

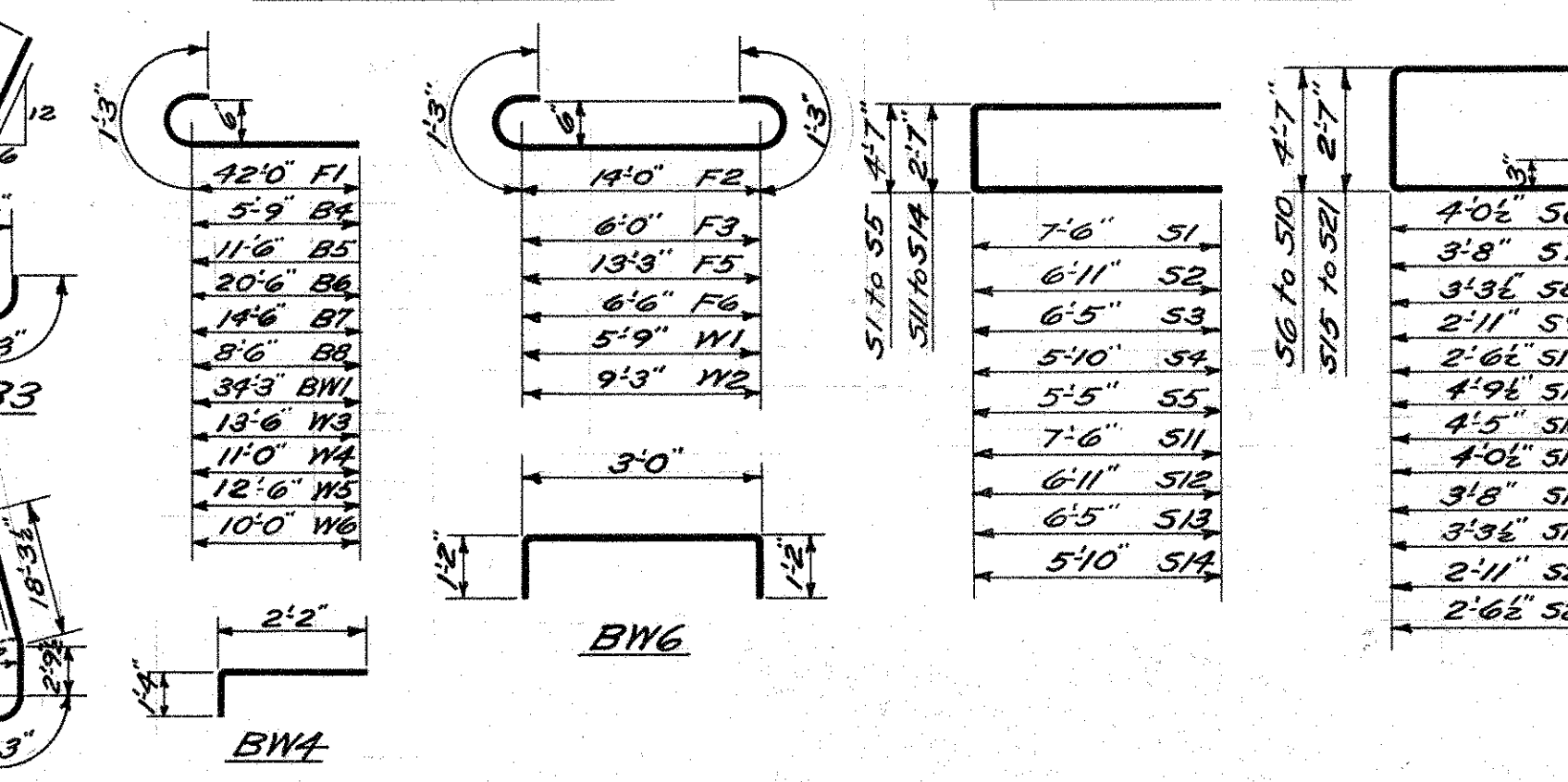
CONNECTICUT
STATE HIGHWAY DEPARTMENT
HOUSATONIC RIVER BRIDGE
RELOCATION U.S. ROUTE NO. 6
BETWEEN
NEWTOWN AND SOUTHBURY
PIER NO. 3

DESIGNED BY F.J.T.
SCALES 1/8" = 1'-0" PROJECT NO. 96-49-03
MADE BY G.A.H. Traced by J.J.B. DATE 10-16-50 BRIDGE SHEET NO. 7 OF 9
CHECKED BY F.J.T. DATE 10-16-50
APPROVED [Signature] DATE 10-16-50



BAR BILL

Mark	Size	No.	Lgth	Weight	Remarks	Mark	Size	No.	Lgth	Weight	Remarks	Mark	Size	No.	Lgth	Weight	Remarks	
F1	1 1/4	232	43'3"	26,791	Detailed	BW5	2 1/4	4	14'0"	37	Detailed	BW5	2 1/4	14	14'0"	16	Straight	
F2	3/4	420	16'-6"	10,409	do	BW6	do	6	6'0"	35	do	BW6	do	6	6'0"	24	Detailed	
F4	1/2	32	3'0"	80	Straight	W1	3/4	124	8'5"	2091	do	W1	3/4	124	8'5"	2091	do	
F5	3/4	124	15'9"	3,992	Detailed	W2	do	108	11'9"	2,594	do	W2	do	108	11'9"	2,594	do	
F6	3/4	124	9'0"	1,616	do	W3	do	64	14'9"	1,418	do	W3	do	64	14'9"	1,418	do	
F7	1/2	44	15'6"	456	Straight	W4	do	64	11'3"	812	do	W4	do	64	11'3"	812	do	
B1	1 1/4	64	25'4"	4,328	Detailed	W5	do	40	13'9"	574	do	W5	do	40	13'9"	574	do	
B2	3/4	40	13'6"	811	do	W6	do	36	11'3"	422	do	W6	do	36	11'3"	422	do	
B3	do	24	10'3"	369	do	V1	do	88	11'0"	647	Straight	V1	do	88	11'0"	647	Straight	
B4	do	16	7'0"	168	do	V2	do	72	8'0"	335	do	V2	do	72	8'0"	335	do	
B5	do	32	12'9"	613	do	V3	do	76	10'0"	5'8	do	V3	do	76	10'0"	5'8	do	
B6	do	16	21'9"	523	do	H1	do	112	15'7"	11'66	do	H1	do	112	15'7"	11'66	do	
B7	do	16	15'9"	379	do	H2	do	24	10'8"	171	do	H2	do	24	10'8"	171	do	
B8	do	48	9'9"	703	do	H3	do	12	9'3"	74	do	H3	do	12	9'3"	74	do	
B9	do	8	17'5"	93	do	H4	do	40	7'9"	207	do	H4	do	40	7'9"	207	do	
B10	do	8	18'5"	98	do	H5	do	24	3'6"	56	do	H5	do	24	3'6"	56	do	
B11	do	8	17'5"	93	do	H6	do	24	4'10"	77	do	H6	do	24	4'10"	77	do	
B12	do	16	15'5"	165	do	H7	do	48	5'6"	156	do	H7	do	48	5'6"	156	do	
																Total	72,861	#



Fed. Aid Proj. FI-4(8)

CONNECTICUT STATE HIGHWAY DEPARTMENT
HOUSATONIC RIVER BRIDGE
RELOCATION U.S. ROUTE NO. 6
BETWEEN
NEWTOWN AND SOUTHURY
NORTH & SOUTH ABUTMENTS

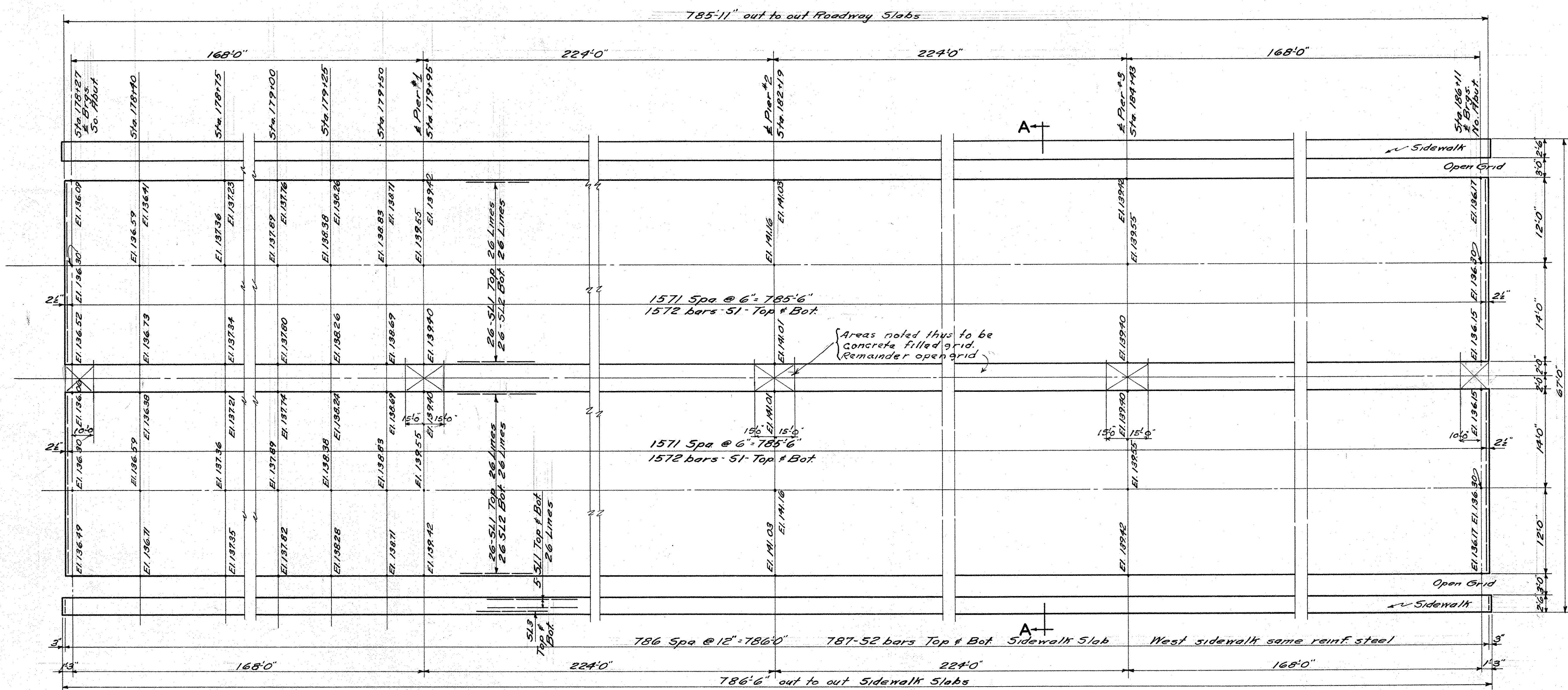
DESIGNED BY F.J.T.
MADE BY F.J.T. Tr. by W.F.D. DATE 6-23-50
CHECKED BY F.J.T. DATE 7/14/50
APPROVED J.D. Dwyer DATE 7/24/50

PROJECT NO. 96-49-03
BRIDGE SHEET NO. 16 OF 19

REVISIONS

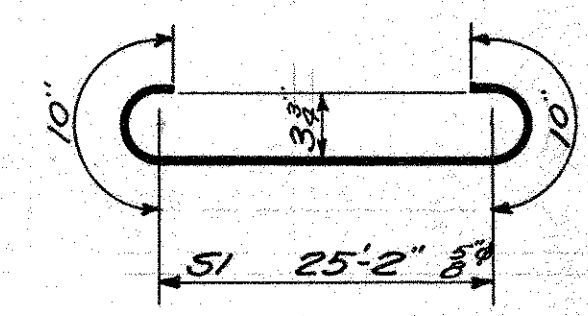
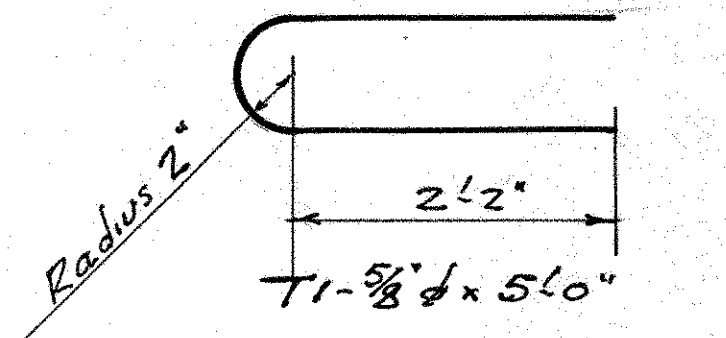
NO.	DATE	DESCRIPTION
1	3/18/51	Width Main Buttresses from 4'0" to 6'0"
2	9/11/51	Place length in wings. Rev. Wings and butt. but.
3	9/25/51	Friction piles on all wings instead of bearing piles.

PUB. ROAD DIV. NO.	STATE	TOWN	PRO. AID PROJ. NO.	PROJECT NO.	YEAR	ROUTE NO.	SHEET NO.	TOTAL SHEETS
1	CONN.	NEWTOWN-SOUTHBURY	FI-4(8)	96-49	1950	6	17	25

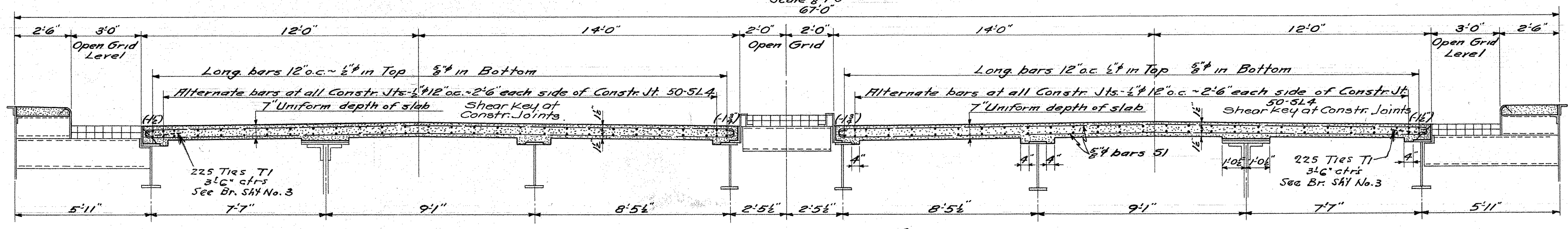


STATION	BANKS	
	Lt.	Rt.
177+50	-6	+6
178+0	-3 1/2	+3 1/2
+27	-2 1/2	+2 1/2
+40	-2 1/2	+1 1/2
+75	-1 1/2	-0 1/2
179+0	-1 1/2	-0 1/2
+25	-1 1/2	-1 1/2
+50	-1 1/2	-1 1/2

NOTE: These banks are applied 12'0" Rt. & Lt. of Lanes.



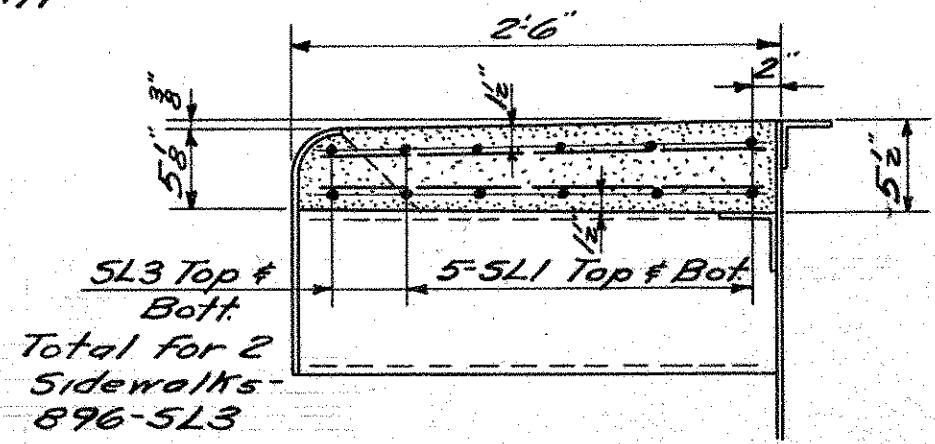
PLAN
Scale 1/4" = 1'-0"
67'0"



SECTION A-A
Scale 3/8" = 1'-0"

(Normal Cross slope) between Sta. 179+50 and Sta. 186+11

NOTE: The contractor shall submit for approval a sequence of placing sections of deck slab so designed as to eliminate tension in the concrete over the supports and at the same time hold to a minimum, deflection and differential distortion in the main girders.



SIDEWALK SLAB
DETAIL
Scale 1" = 1'-0"

Mix	No.	Size	Length	Weight	Remarks
51	6288	3/4"	26'10"	175,981	Detail
52	3148	1/2"	2'4"	4,906	Str.
5L1	1872	40	32'2"	40,225	"
5L2	1352	3/8"	32'8"	46,068	"
5L3	896	1/2"	3'3"	1,945	"
5L4	500	40	5'0"	1,670	"
T1	450	5/8"	5'0"	2,347	Detail
Total				273,139	Lbs.

Fed. Aid Proj. FI-4(8)
CONNECTICUT
STATE HIGHWAY DEPARTMENT
HOUSATONIC RIVER BRIDGE
RELOCATION U.S. ROUTE NO. 6
BETWEEN
NEWTOWN AND SOUTHBURY
CONCRETE DECK DETAILS

REVISIONS		
NO.	DATE	DESCRIPTION
1	2-26-50	ELEVATIONS AT PIERS 1, 2, 3 AND NORTH REINFORCEMENT

DESIGNED BY F.J.T.
SCALES As noted
MADE BY H.E.J. Tr. by W.F.D. DATE 6-22-50
CHECKED BY F.J.T. DATE 1/11/50
APPROVED J.D. Drury DATE 2-24-50

PROJECT NO.	96-49-03
BRIDGE SHEET NO.	9 OF 9